

# CONFEDERATION BOULEVARD GUIDELINES

MANAGEMENT AND STEWARDSHIP OF OUR CAPITAL LEGACY



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## ACKNOWLEDGEMENTS

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**Confederation Boulevard Guidelines - Management and Stewardship of our Capital Legacy** was prepared by the National Capital Commission's (NCC) Real Estate Management Design and Construction Branch (REMDC), Design and Construction Division (D&C), Landscape Architecture and Urban Design Section (LA&UD).

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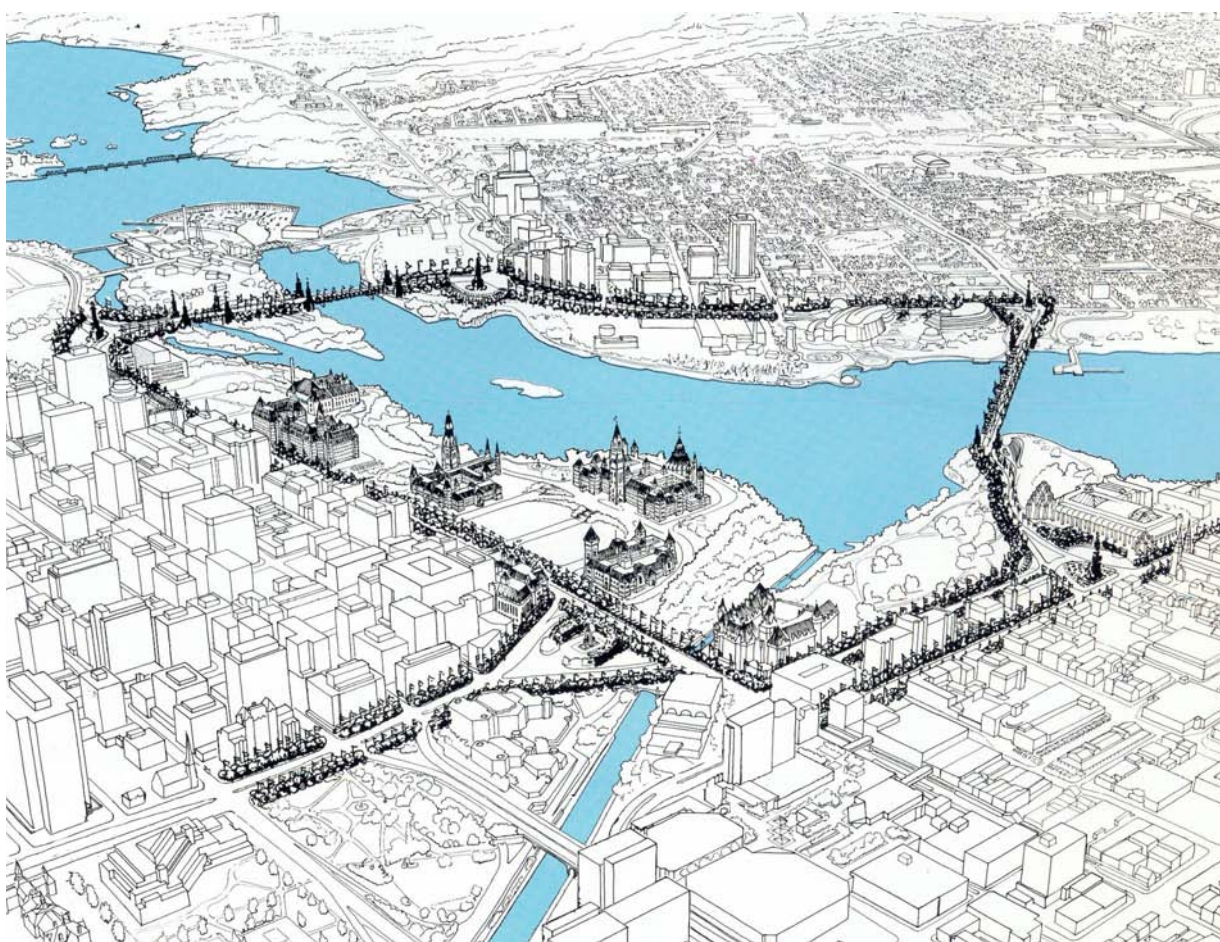
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# CONFEDERATION BOULEVARD GUIDELINES

## MANAGEMENT AND STEWARDSHIP OF OUR CAPITAL LEGACY



Ottawa, 2011



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## FOREWORD

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Confederation Boulevard is a key feature of the Capital Core that is not only closely identified with the National Symbols, but also widely recognized as a destination in its own right. The Boulevard establishes the defining image of federal institutions set in a river-oriented landscape; it distinguishes between the federal and civic; it identifies central Ottawa and Gatineau as a singular federal entity; it is the preferred setting for commemoration and ceremony; and, it accommodates a wide range of activities, both programmed and casual.

The *Ceremonial Routes* report (NCC, 1983) established the planning foundation for Confederation Boulevard. It described the purpose of the key components - Grand Esplanade, links, nodes and gateways - and set out their basic forms and relationships. Implemented over many years and at considerable cost, Confederation Boulevard today clearly reflects the principles and strategies set out in the original plans, and more than achieves the original goals and objectives.

The continuing preservation, evolution and management of Confederation Boulevard is the responsibility of the National Capital Commission and its partners. While the Boulevard is today a powerful emblem of National pride and identity, its survival over the long term will require the concerted efforts of many who will share a clear vision of what it is and what it should become.

Confederation Boulevard is both a national asset to be protected and preserved, and a living environment that will evolve over time. *The Confederation Boulevard Guidelines, Management and Stewardship of our Capital Legacy* will guide the continuing protection of the Boulevard and ensure that the only changes implemented are those that support and reinforce its essential character and purposes. This document will, itself, evolve over time as the emphasis shifts from planning and development to maintenance and management. The basic form, character and purposes of Confederation Boulevard should, however, be held constant.





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## KEY PRINCIPLES FOR CONFEDERATION BOULEVARD

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### ***A Memorable Image***

***Confederation Boulevard projects an image that is dignified, unique and lasting, and that is reflective of Canadian values, heritage and achievements.***

### ***A Vibrant Public Place***

***Confederation Boulevard is a vibrant public place that presents Canadians with a range of opportunities for enhanced community, intellectual and emotional experience.***

### ***Pedestrians First***

***Confederation Boulevard gives first priority to the comfort, safety and enjoyment of pedestrians, and then to the accommodation of cyclists, public transit and other vehicles.***

### ***Universal Accessibility***

***Confederation Boulevard is accessible to all persons, and shall extend accessibility to those beyond the National Capital through a range of communication media.***

### ***Sustainability***

***Confederation Boulevard demonstrates leadership in environmental sustainability and stewardship.***

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## CONCLUSION



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## PART 1 – A BOLD IDEA

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***A Capital Asset  
Planning History  
Implementation  
Current Planning Environment***

Confederation Boulevard was a bold idea that has now come to fruition. It represents the combined thinking and determination of many fine minds over a period of several decades. Today, Confederation Boulevard provides meaning, enjoyment and stimulation to many and will continue to evolve and be appreciated for generations to come.

This section provides a brief history of the planning and development of Confederation Boulevard, and a summary of its current condition and present-day planning environment.

## 1.1 A Capital Asset

Confederation Boulevard is the expression of a bold idea that was championed by many through the decades, and that has finally been realized through the energy and persistence of many others. It is a bold idea that is well worth protecting and preserving.

The idea of Confederation Boulevard represents the convergence of many strands of thought that revolve about the transformation of a remote lumber town into the capital of a large, new nation embraced by two oceans. These strands of thought reflected high respect for the power and beauty of the natural river landscapes, a strong appreciation for Neo-Gothic architectural motifs, and an allegiance to new town planning practices being imported from Europe at the time.

The distillation of these fundamental ambitions into a coherent planning direction was a gradual process extending over more than a century of incremental development. Confederation Boulevard, the single largest urban design intervention in the history of the Capital, extends the essence of this planning direction to the public realm. Defining the seam between the civic and the federal, and between the natural and the urban, and linking two provinces, Confederation Boulevard is a clear expression of these ideals.

Beyond defining and clarifying the Capital Precinct, Confederation Boulevard serves many important purposes. The Boulevard serves as the most important ceremonial route in the National Capital and includes the historic route between Rideau Hall and Parliament Hill. It also provides comfortable and dignified connections between the most important cultural, political and legal institutions in the Capital, and provides these institutions with clear and distinguished addresses.

Confederation Boulevard is the venue for a broad spectrum of both planned and casual activities. The Boulevard is the preferred location in the Capital for commemorations of the highest order that celebrate ideals and values of National significance. It is also the preferred venue for those major ceremonies and celebrations that reach out to all Canadians and beyond.

The Boulevard is a commodious place and an efficient movement corridor for visitors and residents alike. As well as being a well-used route linking important destinations, it offers effective vantage points for viewing the river, the landscapes and the National Symbols. It is a comfortable and welcoming place, providing people with the amenities necessary for resting, viewing and wayfinding. Confederation Boulevard is also a place for the normal, daily activities associated with living, working and moving around an energetic urban environment. It offers the broad sidewalks, convenient public transit, and efficient roadways necessary to move easily around the city; it also provides ready access to the broad assortment of services and amenities that support daily urban life.

The high quality of design and materials that defines Confederation Boulevard have produced an image that is at once attractive and unique, and readily identifiable by most Canadians. The image of the Boulevard exhibits a positive balance between unity and diversity. The visual unity of the Boulevard stems from generous dimensions and the use of a unique and coordinated palette of materials, components and furnishings. The diversity stems from the different ways in which the standard palette has been adapted to the particular environments and site conditions through which the Boulevard passes.

The continued viability of Confederation Boulevard as an important National asset will require continued diligence and dedicated stewardship. While change is inevitable, the vision of Confederation Boulevard as a dignified, distinctive and identifiable complement to the National Symbols must be maintained as the benchmark, and not be allowed to succumb to arbitrary or unconsidered dilution. This is the challenge for the Confederation Boulevard of the future.





*Honour and dignity*



*Colour and delight*



*Vigour and vitality*



## 1.2 Planning History

### 1.2.1 Foundation of the Idea

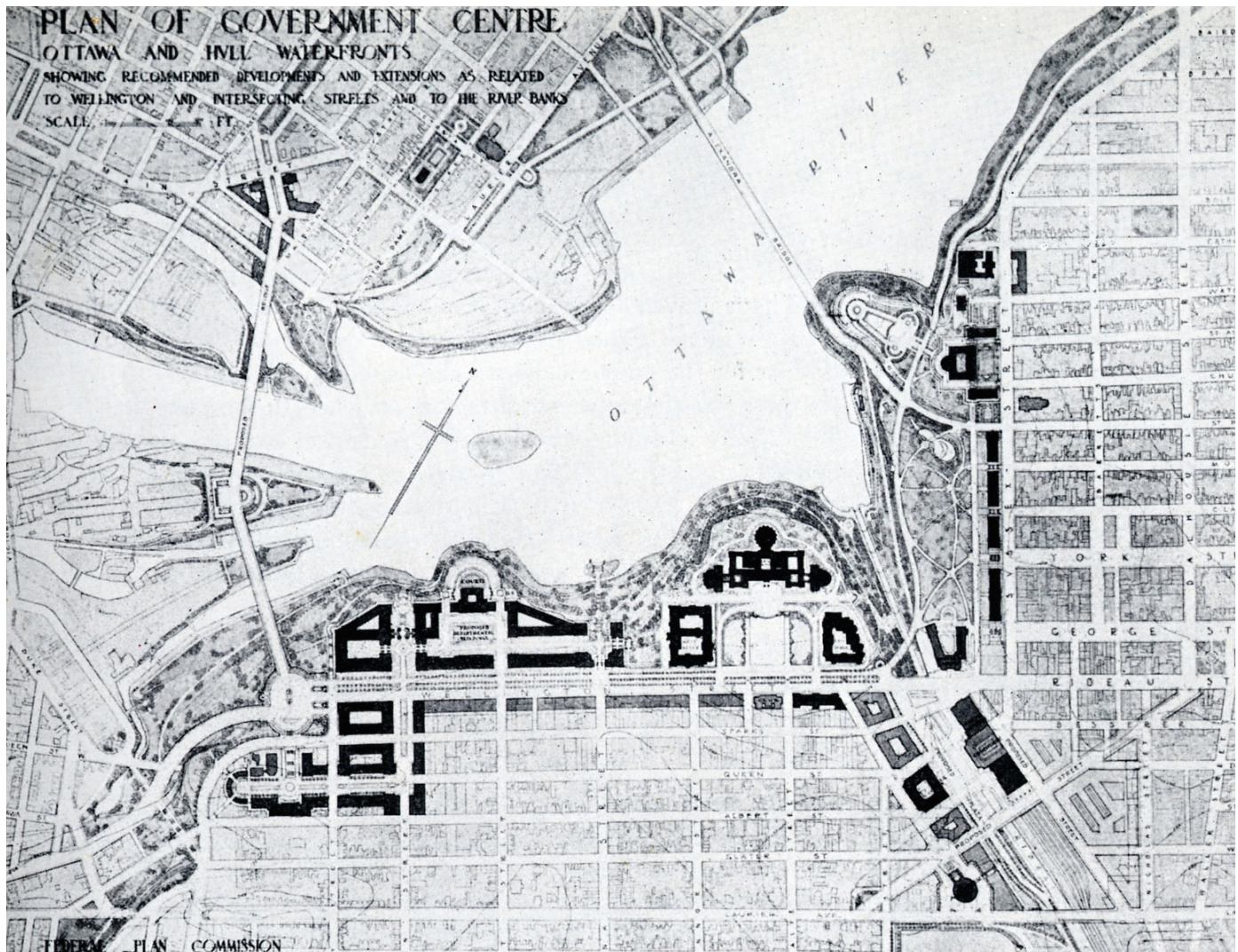
The planning ambitions that would ultimately result in the creation of Confederation Boulevard began to emerge very early in the history of the National Capital.

Developed from humble roots as a lumber town, the National Capital was the subject of many planning initiatives that sought to imbue it with the sense of presence and dignity deemed appropriate for the federal seat of government. A theme that was common to all of the plans was the striking juxtaposition of dignified building ensembles set against, first, a rugged river

landscape, and second, the ordered streets and blocks pattern of the built-up town.

A second important theme was the development of prestigious routes for ceremony and celebration, and the accompanying development of significant connections between Ottawa and Gatineau (formerly Hull).

The Holt Commission Plan of 1915 clearly imagines Wellington Street as a celebrated avenue in front of the federal precinct coupled with significant connections to Hull. Greber's 1938 plan continued to develop this scheme with his 1950 plan including a major Hull connection. Many of these early plans illustrated a major node at the western end of Wellington Street marking the connection over to Hull.



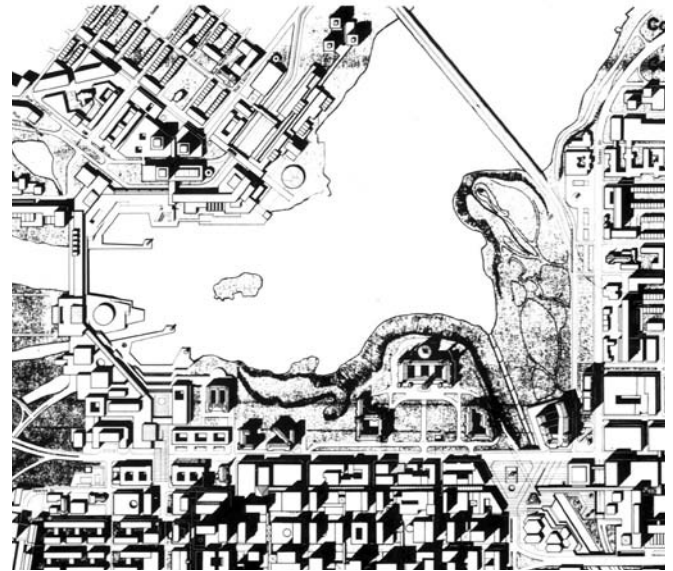
Holt Commission Plan, 1915



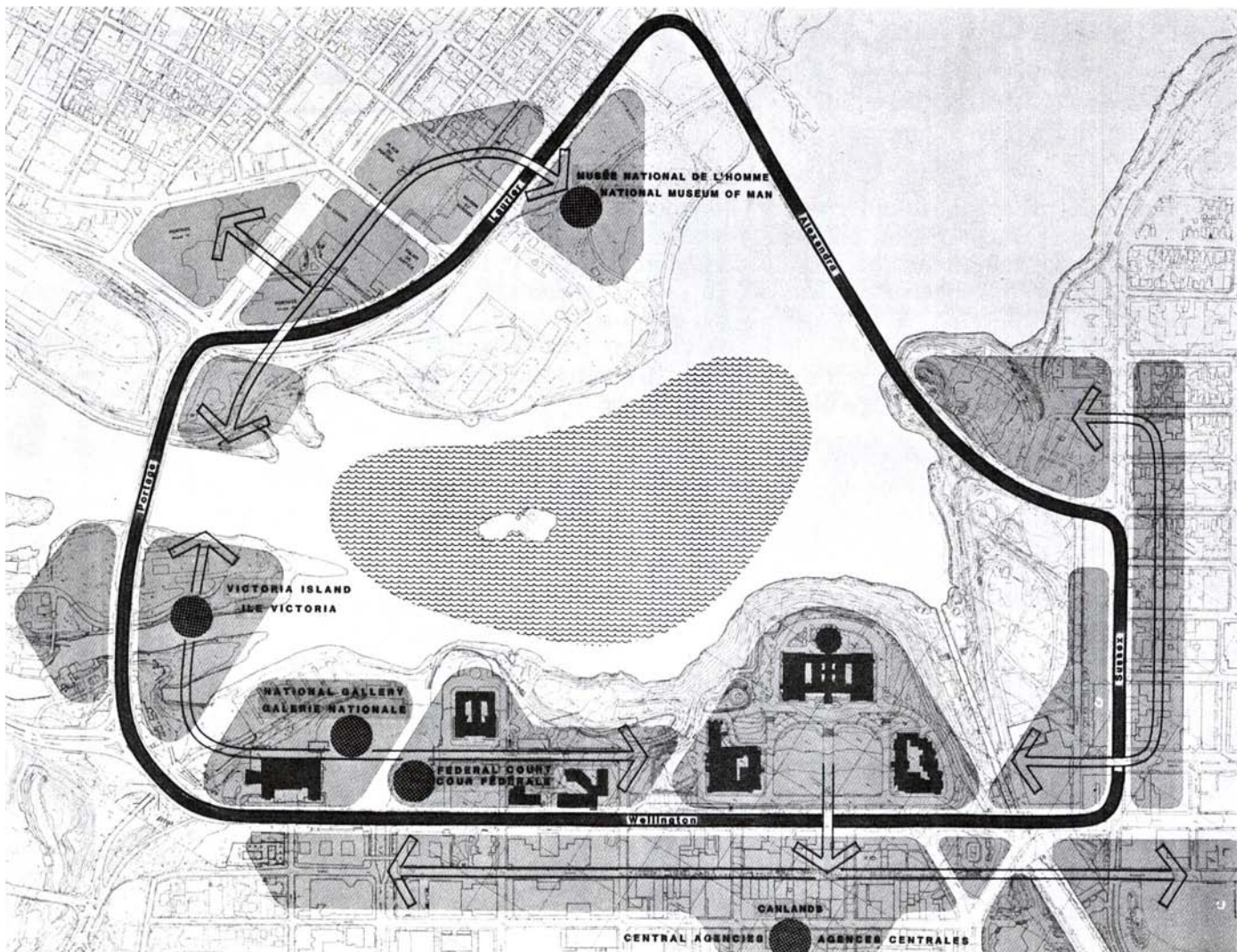
### 1.2.2 Boulevard Canada

Following 1967, enhanced interest in the image and reality of the National Capital led to the formal recognition of Boulevard Canada, which encapsulated the concept of one core linked by a ceremonial circuit.

Studies that formed the basis for this concept included the **Ottawa Central Area Plan** of 1969, the **National Capital Core Area Plan** of 1971 and the **Ottawa-Hull Core Area Waterfront Plan** of 1981. This latter plan clearly set out a vision of a unified Capital Core focused on the river. A ceremonial circuit links the surrounding terrain and defines a federal precinct with major institutions in a landscape setting.



Ottawa Central Area Plan, 1969



The Ottawa-Hull Core Area Waterfront Plan, 1981

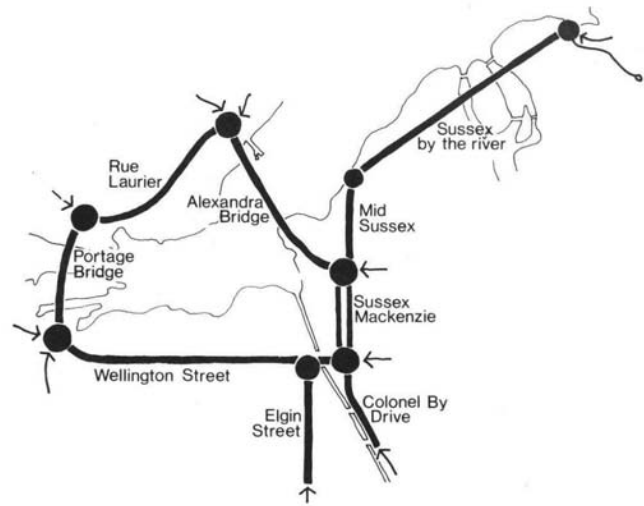


### 1.2.3 Ceremonial Routes, 1983

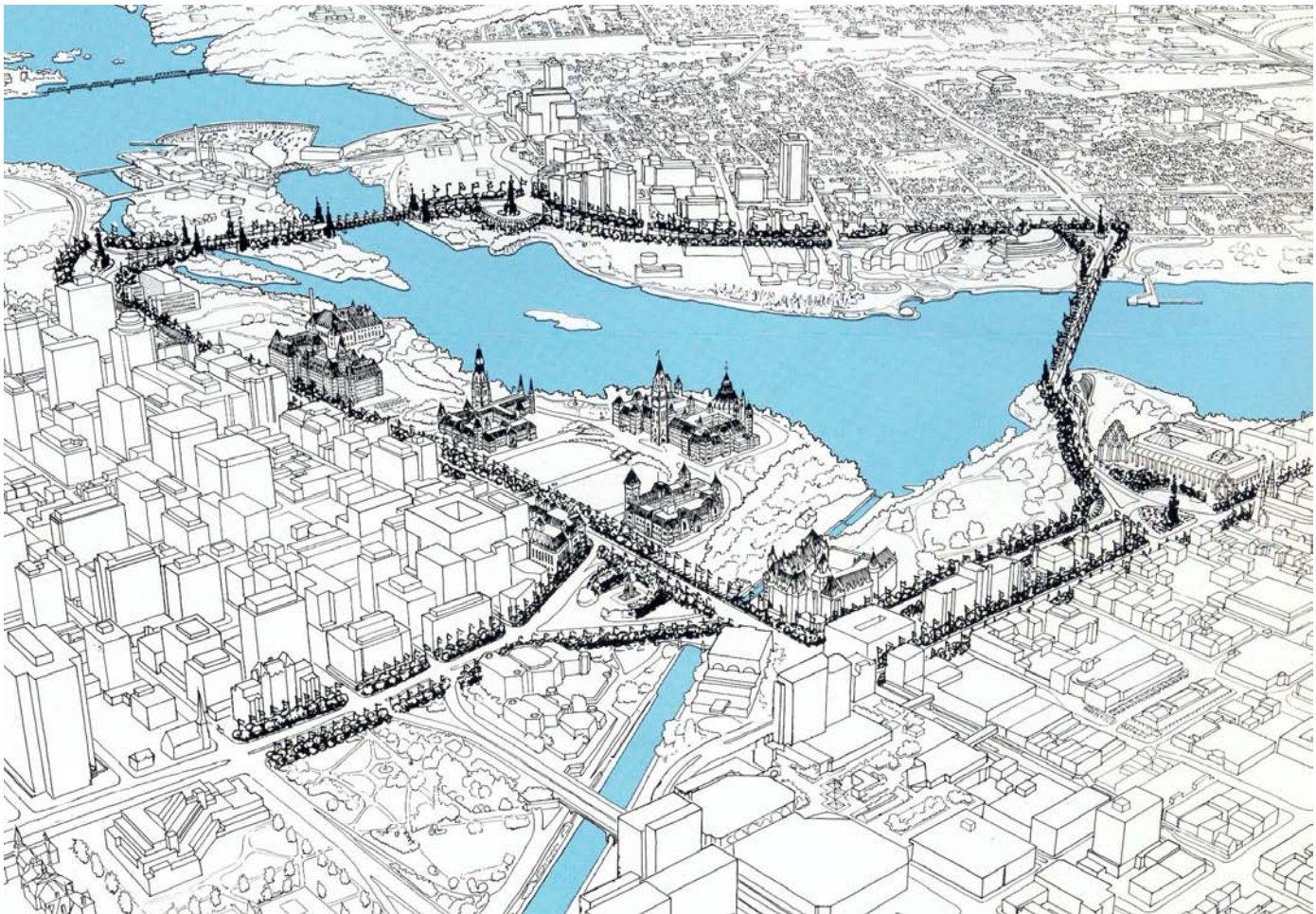
Following acceptance of the idea of Boulevard Canada, a report, **Ceremonial Routes**, was prepared for the National Capital Commission to provide specific planning direction to the design and implementation of Confederation Boulevard.

The report's dual objectives were to celebrate existing ceremonial routes in the National Capital, and to support the perception of a combined Ottawa/Gatineau core as the National Capital. The report crystallized principles and objectives that had appeared in various planning initiatives over the decades to achieve these goals.

The principles and strategies set out in this report are still relevant today and should govern all planning and design decisions. These principles and strategies are discussed more thoroughly in Part 2, Key Components.



Components of the Ceremonial Routes: links and nodes



Bird's-eye View of the Confederation Boulevard Concept



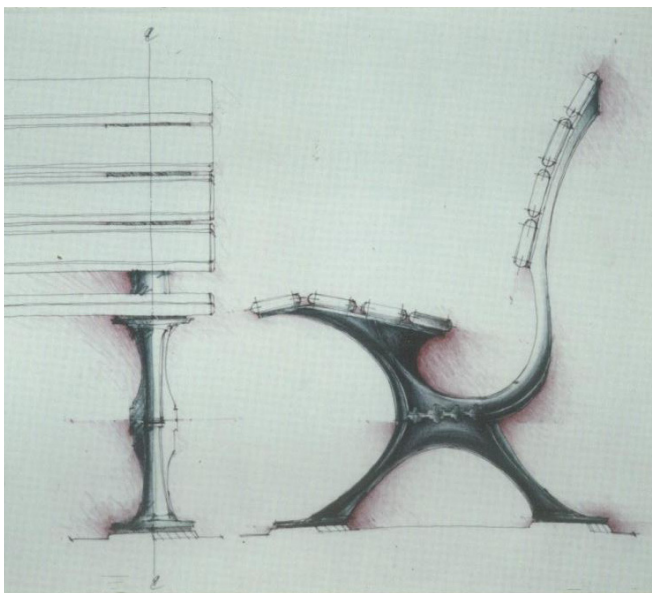
**1.2.4 Confederation Boulevard, 1985**

As the planning of Confederation Boulevard progressed, a series of design and technical studies were undertaken to explore various components, and to make more concrete the vision set out in the 1983 **Ceremonial Routes** report. These studies were consolidated into a report titled **Confederation Boulevard**, which was prepared by the NCC.

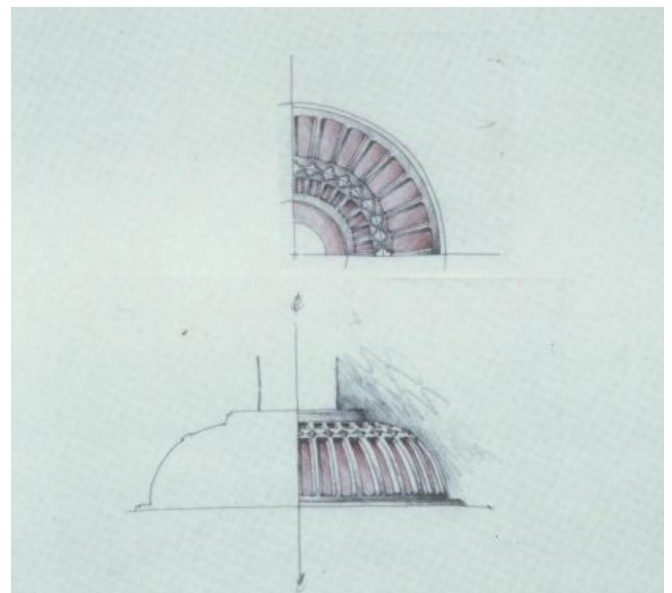
Following confirmation of the founding principles, the report features a demonstration plan that sets out in some detail the design concept for the Boulevard. The report also includes planning and design concepts for lighting, paving, planting and furniture.



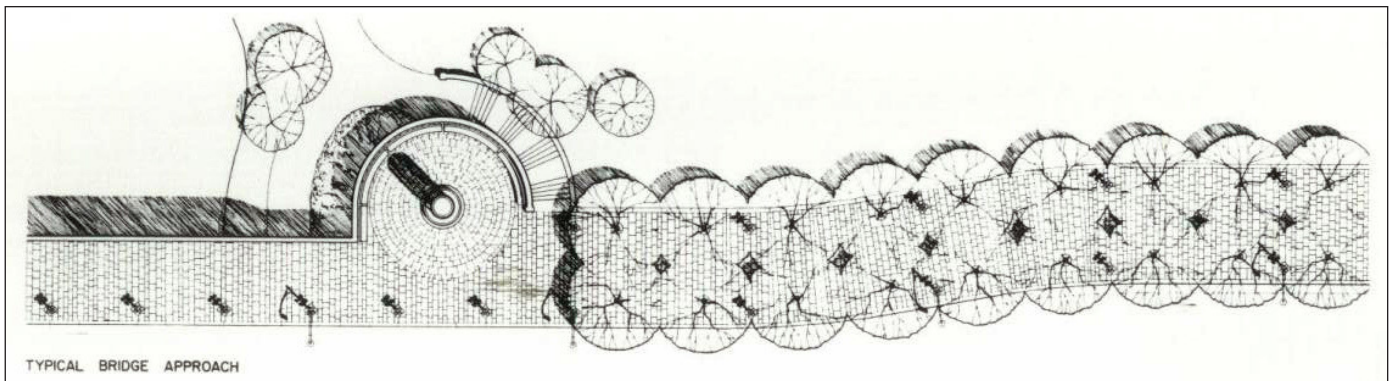
*Demonstration plan*



*Bench concept*



*Tree grate and guard concept*



*Paving concept*



## 1.3 Implementation

### 1.3.1 Phased Implementation

The construction of Confederation Boulevard has been a major undertaking spanning more than 25 years. The first phase, St. Patrick Street, was built in 1987; the final segment, Mid-Sussex Drive, is scheduled for completion in 2012.

Confederation Boulevard was funded by the National Capital Commission, but implementation was often undertaken in collaboration with other agencies or levels of government in conjunction with utility improvements or other projects within the right-of-way. Partners included Public Works and Government Services Canada, the Cities of Ottawa and Gatineau, and the former Regional Municipality of Ottawa-Carleton.

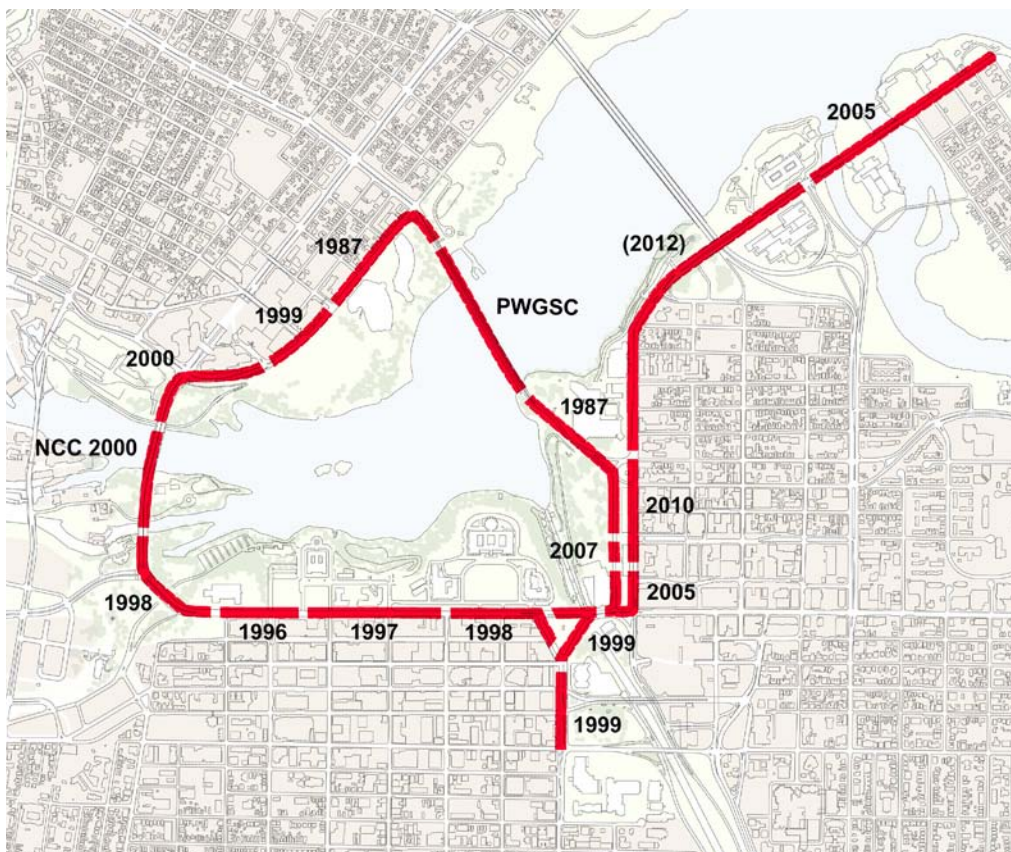
The bridges are slightly different. Portage Bridge is owned by the NCC, which funded and directed its

rehabilitation in 2000 to assume the Confederation Boulevard identity. The Alexandra Bridge is owned and operated by Public Works and Government Services Canada. It has not been redeveloped in the Confederation Boulevard vocabulary with the exception of banners and pedestrian light fixtures along the boardwalk.

### 1.3.2 An Evolving Design

Over the course of the Boulevard's implementation, changes were made to the design of various components to improve performance and longevity.

One major shift involved the change in sidewalk surface treatment from granite unit paving to concrete unit paving. Granite paving, used on the first two phases, St. Patrick Street and in front of the Museum of Civilization, was extremely attractive, but was subject to damage from snowploughs and frost heave. The granite was subsequently replaced by concrete unit paving, which has proven to be attractive and considerably more durable.



Implementation by phases, 1987 - 2012



Other changes included modified light fixture globes on some street lights and the shift in tree surrounds from metal to precast concrete to coarse gravel.

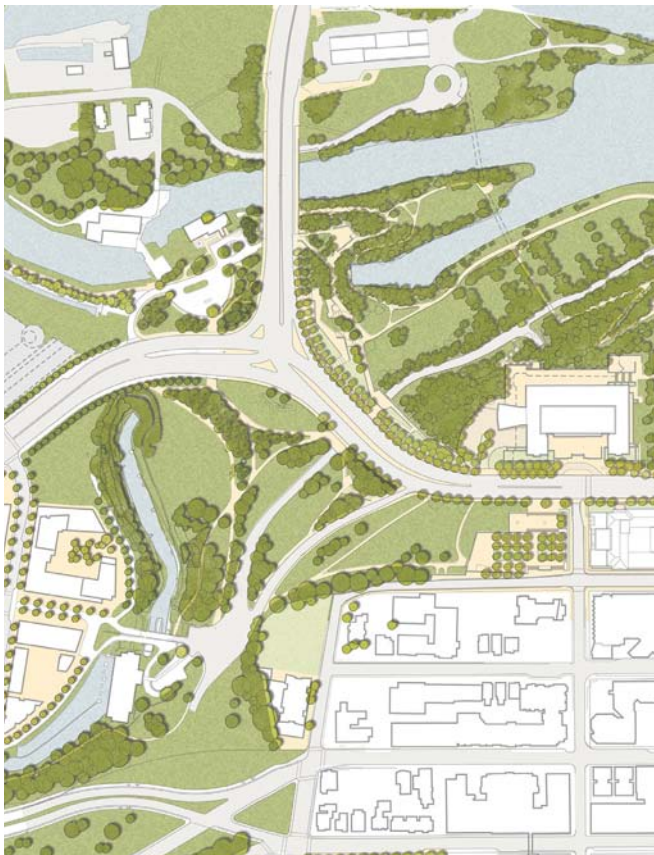
### 1.3.3 Node Development

With development of the links nearing completion, greater attention is now being paid to realizing the full potential of the nodes. The following advances have been made:

- Confederation Square was rehabilitated in 2003;
- The Sussex/St. Patrick node was completed with the development of the Peacekeeping Monument in 2000;
- Nodes at the north end of Alexandra Bridge and at Laurier/Taché have been partially developed although still lack major commemorations;
- A Redevelopment Vision for the Wellington/Portage node was presented to ACPDR in 2007. A schedule for implementation has not been established.



Official opening of the Boulevard by Prime Minister Jean Chrétien, 2000



The existing Wellington/Portage intersection



Redevelopment vision for the Wellington/Portage intersection, 2007







*Reaffirm Confederation Boulevard as the location for the most important political and cultural institutions in Canada, as the Capital's ceremonial route, as the showcase of significant monuments and other symbols, and as the centre of programming and interpretation activities in the Capital.*

At the same time, the **Core Area Sector Plan** recognizes that the Boulevard is still evolving:

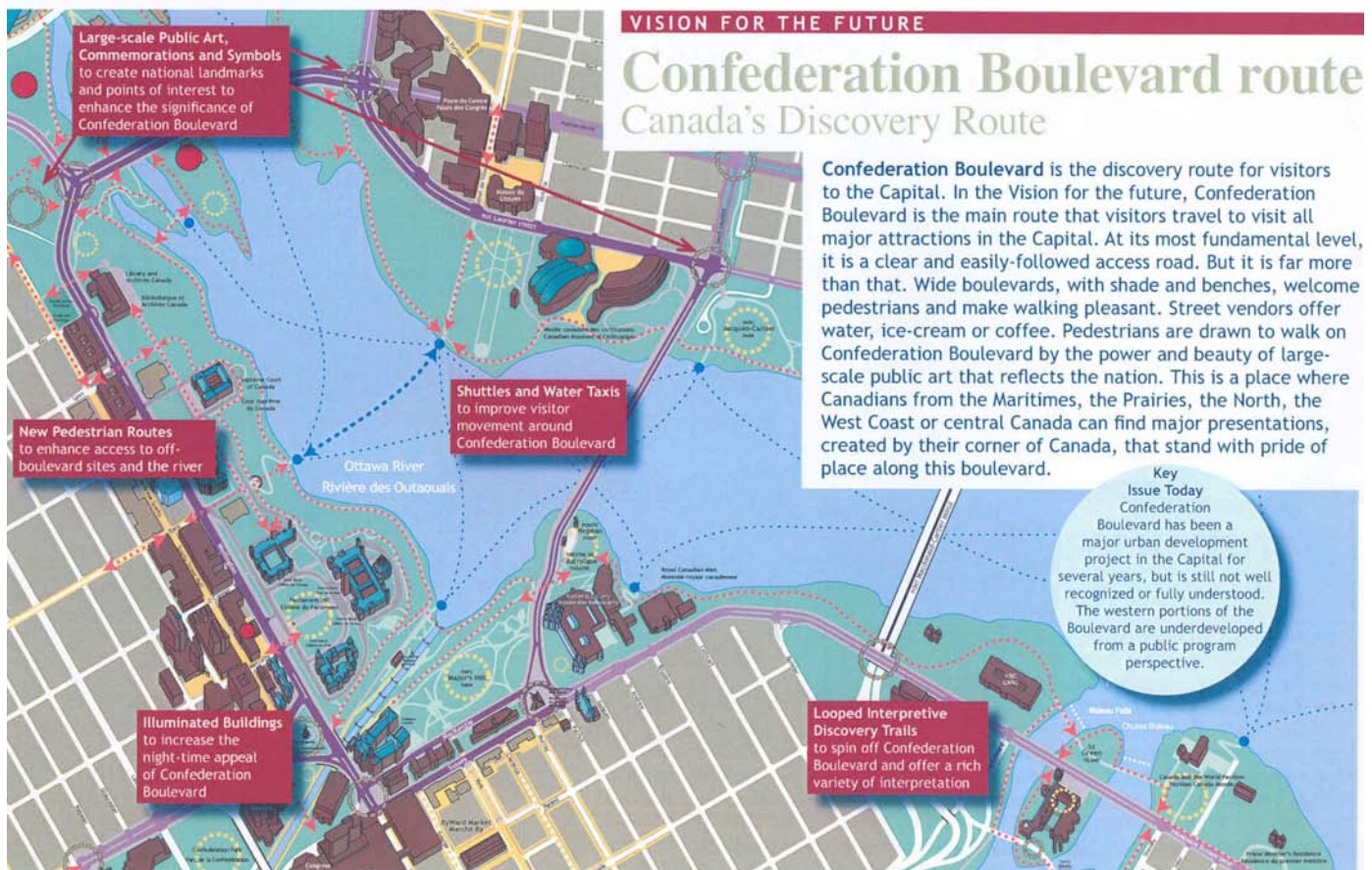
*The Boulevard is not yet balanced as a programming stage. Certain nodes are undeveloped such as the Wellington/Portage, and Portage/Laurier nodes. The eastern area between Parliament Hill and the Museum of Civilization is a busy part of the route for pedestrians because it is dense with attractions, while the western areas of the Boulevard are far less explored by visitors.*

**Reflecting a Nation, NCC Public Programming Strategy** (NCC 2005) sets out ways in which the visitor's experience of the National Capital can be enhanced to create a greater sense of national identity and awareness.

The report recognizes that Confederation Boulevard will become the prime "discovery route" for visitors in the Capital. It will not only provide a clear and comfortable route between destinations of national importance, but it will also in itself express important regional and national values through commemoration, public art and other interpretations of the Canadian experience.

The report also observes that the potential of the Boulevard has not yet been fully realized, and that it is not yet fully animated. The north and western portions in particular require more work to make them lively parts of a coherent whole.

The report recommends developing major commemorations or public art installations at three under-developed nodes: rue Laurier/Alexandra Bridge, Laurier/Taché, Wellington/Portage. It also recommends improving access to and along the water's edge, using shuttles and water taxis to enhance connections across the river, and developing looped interpretive trails to enrich the Boulevard experience.



Reflecting a Nation, NCC Public Programming Strategy, 2005

**Canada's Capital Commemoration Strategic Plan** (NCC 2006) classifies potential commemorative sites in Canada's Capital according to their physical, symbolic and urban characteristics. Sites along Confederation Boulevard are assigned the highest priority.

The primary categories include:

- **Order One Landmark Sites** - include the seven landmark nodes on Confederation Boulevard, which constitute major points of reference in the urban structure and in the image of the Capital. They are reserved for commemorations to ideas and events of the highest order. The National War Memorial and the Peacekeeping Monument occupy two of these sites.
- **Order One Gateway Sites** - are located at seven key street intersections on the major approaches to Confederation Boulevard. These present opportunities for commemorative installations that are com-

plementary to the landmark nodes. The Canadian Tribute to Human Rights at the foot of Elgin Street is an Order One Gateway Site.

- **Order Two Sites** - include key locations along Confederation Boulevard and in the Capital Core. The commemorative installations on these sites will vary in size and scope according to the surrounding context. These sites are reserved for commemorations to people, events and ideas of national symbolic significance to Canada and Canadians.
- **Order Three Sites** - include intimate settings best suited to smaller scale commemorations, yet still of national symbolic importance.



Capital Commemoration Strategy, NCC 2006



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## PART 2 - KEY COMPONENTS

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***Fundamentals - Ceremonial Routes Report, 1983***  
***A Linking Ring - The Esplanade***  
***The Town Side***  
***The Carriageways***  
***Elgin Street***  
***Sussex Drive***  
***The Nodes***  
***The Gateways***  
***Lighting***

Confederation Boulevard is recognizable and cohesive to the extent that there is a common family of materials and furnishings that are consistently applied. Nevertheless, there are several different major components that combine to create the Boulevard. Each component has a particular character and role, and yet each shows considerable variation in response to local site conditions.

Following a discussion of the fundamental planning principles, this sections outlines the basic character and role of each component, and their relationships to each other.

## 2.1 Fundamentals - Ceremonial Routes Report, 1983

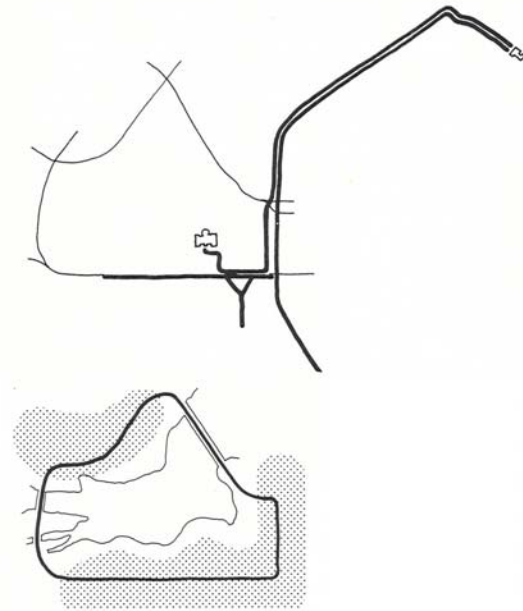
The 1983 report, **Ceremonial Routes**, was prepared for the National Capital Commission to provide the intellectual basis and specific direction for the design and implementation of Confederation Boulevard. The report articulates the basic goals and objectives, and sets out specific design concepts and strategies.

The report is still relevant today and should remain a central planning reference for the Boulevard's continuing development and management.

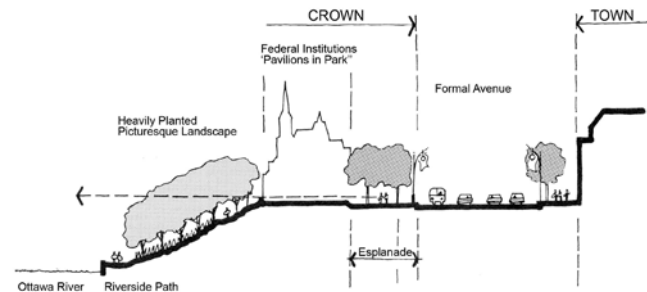
The images on the next few pages, taken from **Ceremonial Routes**, illustrate the key principles underlying the current form of Confederation Boulevard.

### 2.1.1 Town and Crown

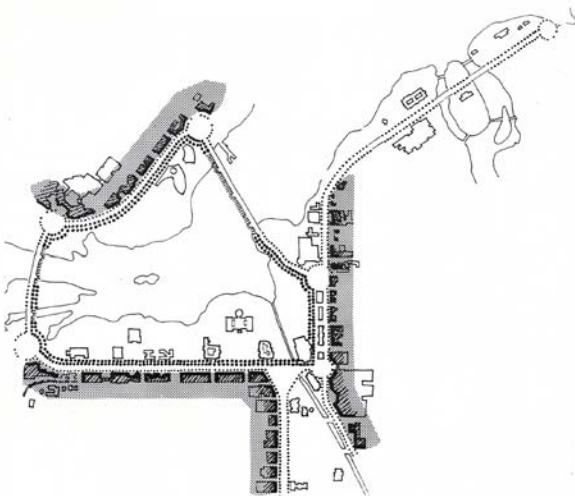
A central strategy was to develop a ceremonial ring to reinforce the image of federal institutions set in a pastoral landscape and distinguished from the ordered urban pattern of the city around. To the extent possible, the ceremonial ring would be lined with a continuous "Town" wall of buildings that would contain and define the federal precinct, the "Crown". The federal institutions are pavilions set in the river-focussed landscape.



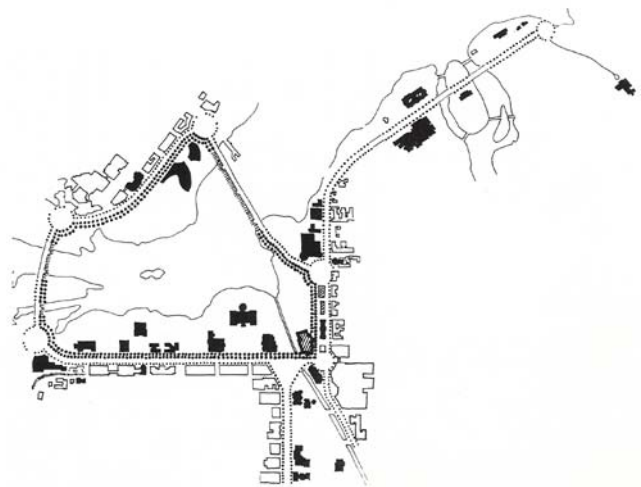
Routes for ceremony/Routes for linkage



A Town Wall containing Crown institutions and open space



A Town Wall containing Crown institutions and open space



Public institutions cluster along the routes

### 2.1.2 The Primary Components

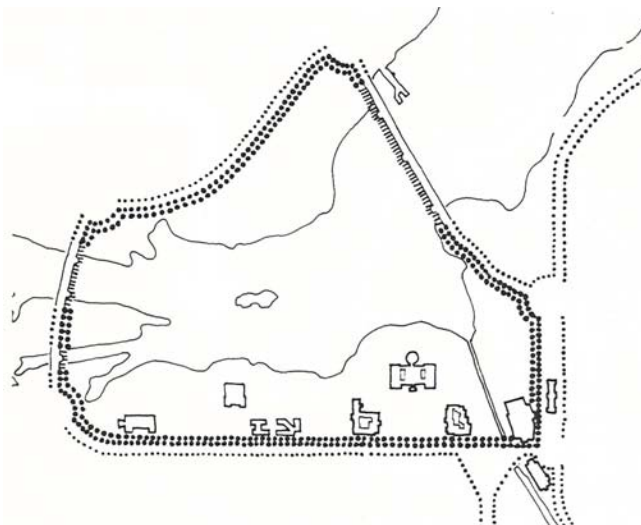
The primary components of the ceremonial ring are designed to reinforce the sense of a continuous circuit around the federal precinct as well as provide a distinguished address for federal institutions.

The **Grand Esplanade** is the broad and richly furnished pedestrian promenade along the inside of a series of separate roads, which are thereby combined to form a distinguishable Boulevard.

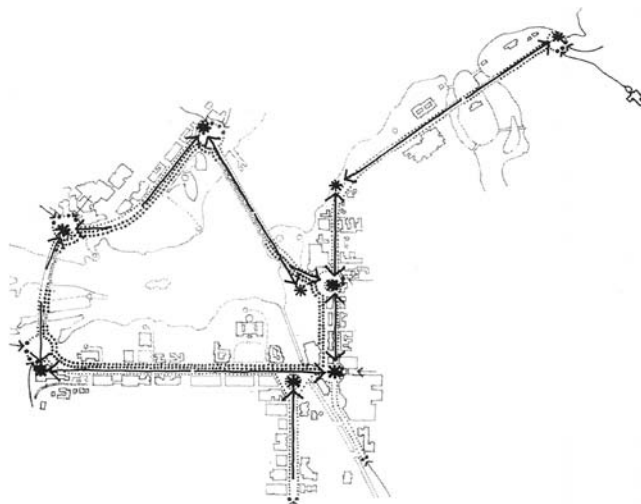
**Links** are the continuous sections of the Boulevard between the nodes.

**Nodes** are highly developed places at the turning points and entrances to the Boulevard that include markers, a sense of spatial enclosure and entry features.

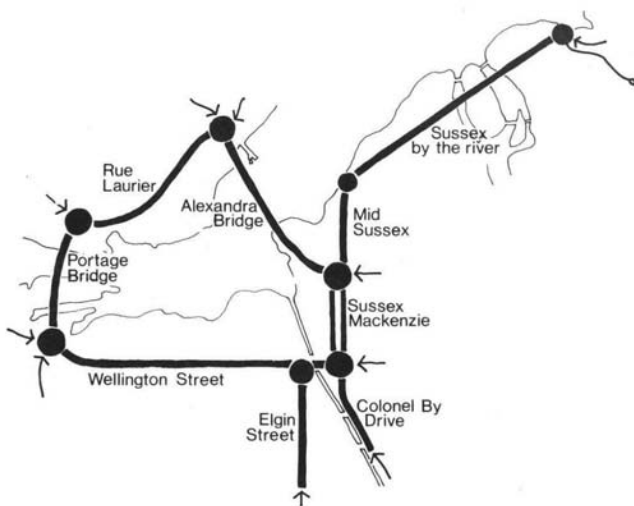
**Gateways** are special features, usually integrated components of the nodes, that signify entry to Confederation Boulevard.



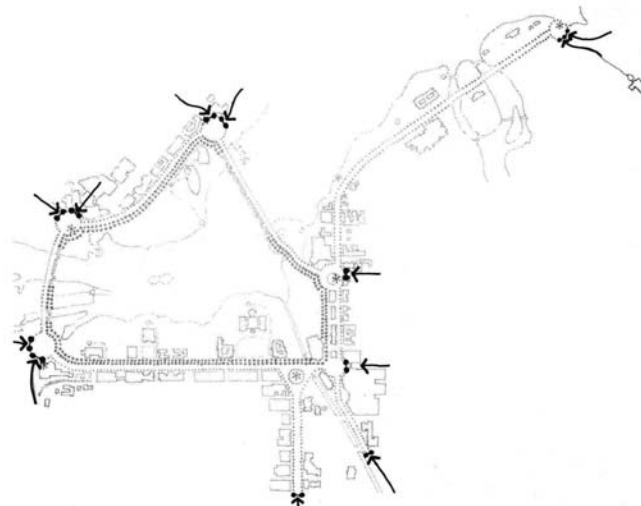
A Grand Esplanade inside the Ring



Markers at the turning points



Components of the Ceremonial Routes: links and nodes



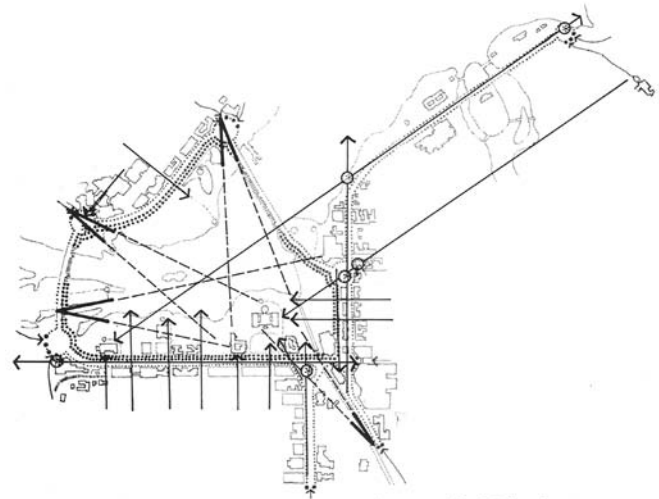
Gateways at the entrances to the Ceremonial Routes

2. Key Components

**2.1.3 Framed Views**

The interaction between the Town and Crown geometries produces dramatic views of the National Symbols and the landscape setting as one emerges from the regular grid of the surrounding urban streets. These views are sometimes fragmentary, other times layered and deep. These views should be protected.

Views from key vantage points along the Boulevard should be developed, particularly from the nodes and gateways. These views would feature the river and its associated landscapes, and important existing and developing landmarks.



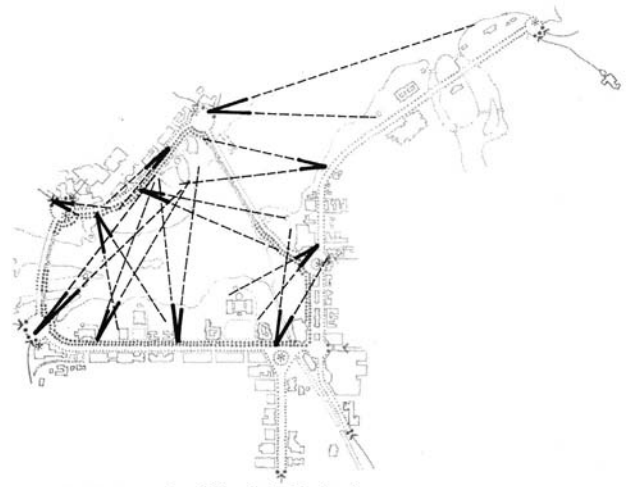
*Framed and layered views to be preserved and enhanced*

**2.1.4 Furnishings**

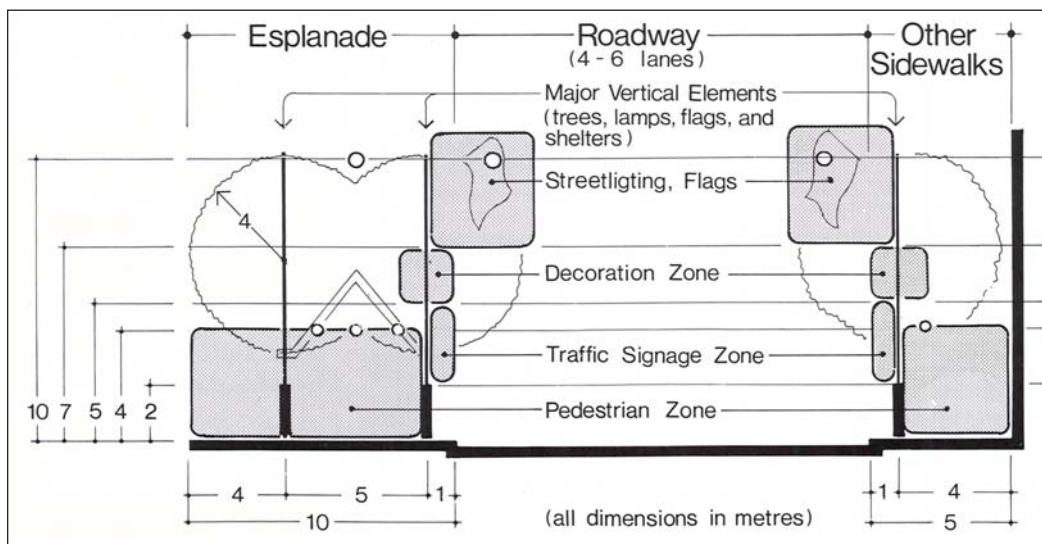
Furnishings - pavements, lights, banners, trees, signage, benches and kiosks, etc. - together play a crucial role in lending the Boulevard a degree of visual cohesion, and in creating dignified ceremonial avenues which are clearly different from other nearby city routes.

The report recommends that the various elements be carefully organized along the street both in cross-section and in plan to create an ordered hierarchy of elements that work together.

The report also emphasizes that streetscape elements should be custom-designed in order to ensure that Confederation Boulevard is both dignified and unique.



*Framed views to be developed*



*Vertical zones for streetscape furnishing functions*





*Ceremonial Routes demonstration plan, 1983*



## 2.2 A Linking Ring - The Esplanade

The central component of Confederation Boulevard is a linking ring made up of area streets. The primary purpose of the linking ring is to connect Ottawa and Gatineau and create a singular federal precinct focused on the river landscapes. This federal precinct is the Crown Territory that features pavilion buildings set in a pastoral landscape setting.

The Grand Esplanade is the primary device for establishing the continuity of the ring as it passes along the various streets and intersections that make up the Boulevard. It enhances the public presentation of the institutions and major landscapes inside the ring, it offers a comfortable and honourable way for pedestrians to move between destinations along the ring, and it provides a range of vantage points for viewing buildings, landscapes and the river.

The Esplanade consists of a broad, paved promenade located at the edge of the Crown territory and adjacent to Boulevard streets.

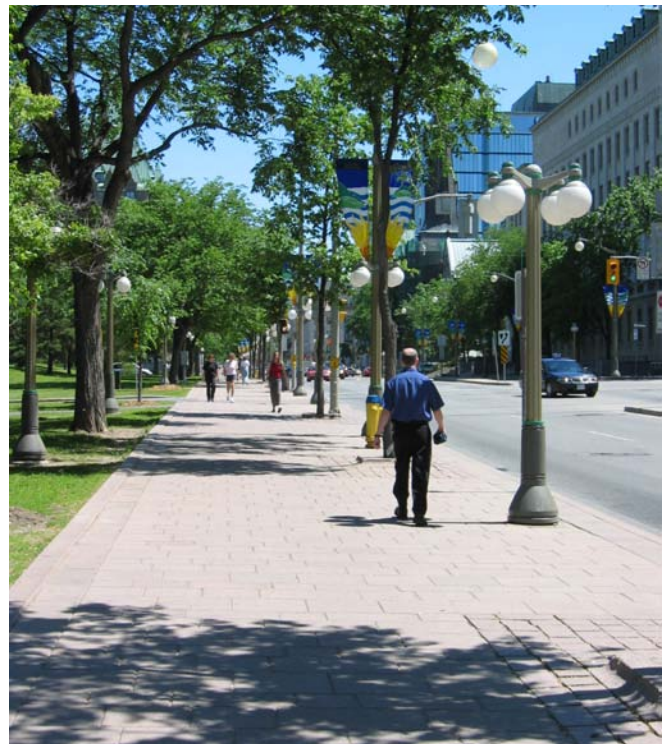
The 2.0 meter zone adjacent to the roadside curb is relatively consistent in design and treatment. It is here that are located lights, signage, bus shelters, kiosks, hydrants, and other elements. Trees are also located in this zone where conditions permit.

In two locations, on St. Patrick Street and Portage Bridge, a two-way bicycle path was built between the Esplanade and the roadway. This was a site-specific measure necessary to provide safe cycling linkages across the two interprovincial bridges. Elsewhere on the Boulevard, cyclists will be accommodated on the roadway and not at the expense of the pedestrian realm.

The treatment of the inside edge of the Esplanade varies in response to the different site conditions found all along the ring. Typical conditions include lawns along Wellington Street, steps and retaining walls along Major's Hill Park, the retaining wall along St. Patrick Street, the sunken court in front of the Museum of Civilization, planters and railings at Laurier/Taché, and the railing on Portage Bridge. The variety of inside conditions is balanced by the high degree of consistency along the curb side.



*A Linking Ring connecting two provinces and defining the Crown Territory*



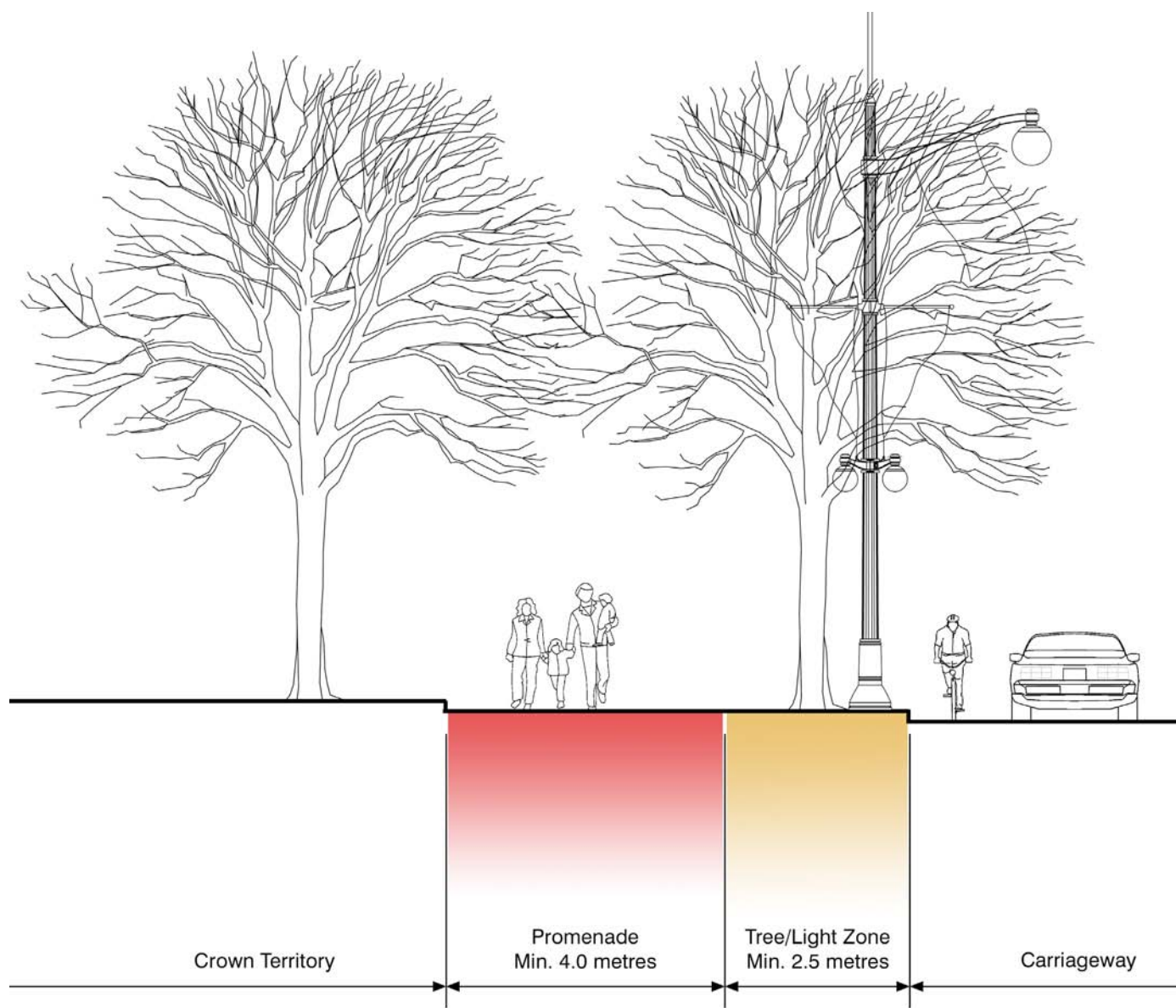
*The Grand Esplanade: Wellington Street*

Of all the Boulevard's primary components, the Esplanade receives the highest level of development and maintenance. High quality materials, finishes and furnishings are critical to conveying the importance and stature of the Esplanade, and also critical to reflecting the visual and symbolic significance of the buildings and landscape of the Crown territory. Care should always be taken to ensure that only materials and furnishings are selected and employed that support this role.

The location of the existing curb should not be altered. A key feature of the Esplanade that should be pre-

served is the generous width and relative openness of the main walking surface. The purpose of this is to not only accommodate crowds when they occur, but to also impart a sense of grandeur commensurate with the importance of the adjacent buildings and landscapes. The width of the promenade also provides many long distance views along the length of the Esplanade.

The central promenade of the Esplanade should be kept free of elements that will impede movement, reduce the sense of grandeur or impact views along its length.



*The Grand Esplanade: Minimum Desirable Dimensions*



## 2.3 The Town Side

The sidewalks and buildings opposite the Grand Esplanade define the Town side of Confederation Boulevard. The lights and banners of the Town sidewalks echo those of the Esplanade, creating a visually balanced, double-sided boulevard.

Like the Esplanade, the lights, banners, signage and trees are adjacent to the curb, leaving a generous walking surface beyond. Primary materials are poured-in-place concrete with a granite curb and border. Trees are planted where conditions permit and are set off with granite tree surrounds.

Where flanked by buildings, the Town sidewalk extends right to the building face. Where flanked by lawn or plantings, the sidewalk is treated with a plain edge.

The Town sidewalks are developed with more basic materials (concrete) to emphasize the greater importance of the Esplanade across the street. The monochromatic nature of concrete is also a suitable foil for the many different architectural expressions found on the Town side.

Nevertheless, even basic materials, if installed and maintained with care, can be quite attractive and appropriate for Confederation Boulevard.

The Town sidewalks should be developed as lively social places reflecting the animation characteristic of the urban scene. The Town sidewalks and related plazas are the places where people on the street will find the food, casual entertainment and information necessary to make their visits stimulating and comfortable.



*The Town Side: A continuous street wall*



*Town sidewalks: Wellington Street*



*Town sidewalks: lively and well-served*



## 2.4 The Carriageways

The various city streets that make up Confederation Boulevard are not only multi-modal transportation corridors but also ceremonial avenues. For the most part broad and efficient traffic arteries, the streets are marked as ceremonial by continuous lines of distinctive streetlights on both sides, and by colourful flags and banners.

Emphasis should be placed on moving people rather than vehicles.

Major efforts have been and should continue to be placed on subduing the transportation role (i.e., moving vehicles), and enhancing the ceremonial role (i.e., accommodating people). To the extent possible, priority

should be given to pedestrians and cyclists over vehicles by maintaining broad sidewalks, providing frequent and generous crosswalks, and developing continuous safe cycling facilities. Cyclists should be accommodated on the roadway in designated lanes and not at the expense of the pedestrian realm.



*Wellington Street: Multi-modal transportation corridor*



*Wellington Street: Ceremonial avenue*



## 2.5 Elgin Street

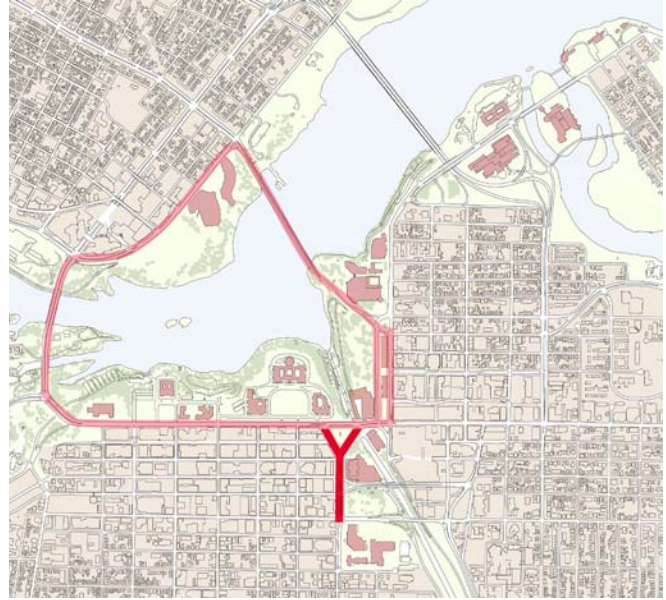
Elgin Street is a unique part of Confederation Boulevard that leads up to the National War Memorial, a powerful visual focus at Wellington Street. Elgin Street is one of the most important ceremonial approaches to Parliament Hill and a venue for significant commemorative ceremonies.

Elgin Street is a very large scale street with a right-of-way width of about 60 meters (200 feet). It accommodates 6 lanes of traffic, a median and sidewalks that are up to 13 meters (40 feet) wide. A street on such a grand scale requires a bold and simple treatment.

The most noticeable features of the Elgin streetscape are dense rows of lindens on either side. These frame the view of the National War Memorial and reduce the scale of the street for pedestrians. The trees are planted in continuous fields of granite cobbles. Generous concrete sidewalks between the trees and building face provide adequate space for pedestrians.

Split-faced granite curbs define the roadway and the medians are paved in granite cobbles. The street is illuminated by standard Confederation Boulevard lights. Tall street lights illuminate the roadway; pedestrian fixtures, located inside the treeline, illuminate the sidewalks.

The Confederation Boulevard treatment on Elgin Street ends at Laurier Avenue, and is marked by Threshold Light fixtures. This gateway termination is quite weak and requires additional work to be effective.

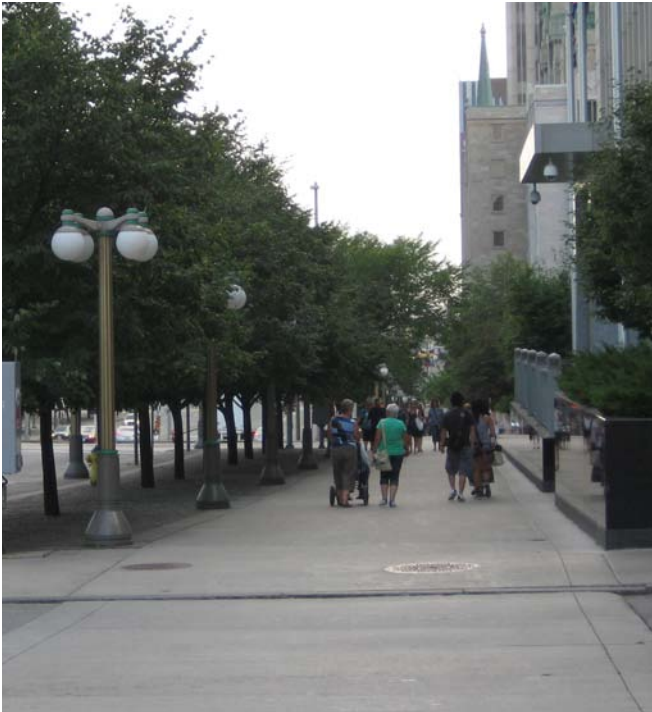


*Elgin Street*



*Elgin Street: Granite cobbles in the median*





*Elgin Street: Concrete sidewalk*



*Elgin Street: Trees in a field of granite cobbles*



*Elgin Street: A powerful focus*



## 2.6 Sussex Drive

Sussex Drive north from the Peacekeeping Monument to Rideau Hall is a broad, large-scaled avenue with buildings usually set back from road. It is equipped with cycle lanes, but is not heavily used by pedestrians.

Sussex Drive is renowned for the number of civic and diplomatic institutions along its length, and for Rideau Hall in particular, which terminates Confederation Boulevard at the north end gateway. Sussex Avenue is famous for the ceremonial procession that occurs between Rideau Hall and Parliament Hill during both the opening of Parliament and its dissolution.

The Boulevard streetscape along Sussex is simple, clean and dignified. The primary streetscape element is the concrete sidewalk with granite curb and border, and continuous rows of Confederation Boulevard lights along each side. The sidewalk is bordered by rows of granite setts where it abuts lawns. In specific locations, flush medians in the roadway are paved in granite cobbles.

The large scale and geometry of Sussex Drive offer a special kind of experience not found elsewhere on Confederation Boulevard. In particular, the long straight stretches of the street north from King Edward Avenue provide long focused views of distant landmarks. This experience is made all that much more dramatic by the continuous rows of lights and banners that line both sides of the street.

The section south of King Edward Avenue is being re-constructed in the summer of 2011.



*Sussex Drive*



*Sussex Drive: Simple and dignified sidewalk treatment*





*Bytown Bridge on Sussex Drive*



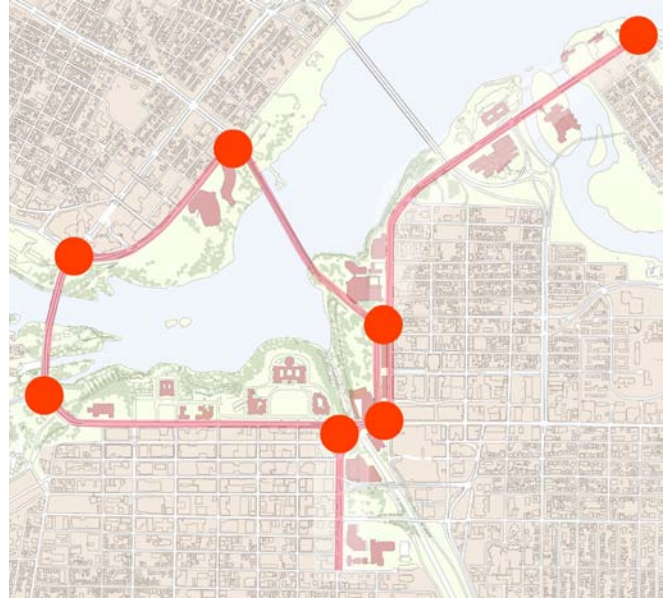
*Sussex Drive: Long, focused views*



## 2.7 The Nodes

Seven nodes have been identified on Confederation Boulevard to mark changes in direction and points of entry. They are intended to have a degree of spatial definition, significant markers and entry features. Nodes have also been identified as the locations for commemorations of national importance.

Development of the nodes is a major undertaking requiring considerable collaboration and resources, and perhaps most important powerful leadership. To date, two of the proposed nodes have been fully developed. Confederation Square was, in fact, in existence when the Boulevard was first being planned and served as a prototype for the node concept. The Peacekeeping Monument node, developed in the 1990s, was propelled by the need to find a prominent site for the commemoration itself. In all likelihood, future node development will similarly be associated with the installation of significant commemorations.



Nodes mark changes in direction and points of entry

*Confederation Square  
View from southeast*

*This location is the most clearly defined node on Confederation Boulevard, and has been cited as the prototype for other nodes. It is a well-defined space at one of the most important points of entry to the linking ring and features a major commemoration, the National War Memorial.*







*Sussex/Wellington Node  
View from southwest*

*This location is a very busy intersection that has not as yet been developed as a recognizable node. Development is constrained by the high traffic volumes and existing buildings. The forecourt to 700 Sussex Drive could have been a component of a new node development except that it has little visual prominence as it is depressed below the level of the surrounding roads.*



*Peacekeeping Monument  
View from northeast*

*This node is the only location developed to its full potential during the development of Confederation Boulevard. The node features a major commemoration with considerable visual presence.*



## 2. Key Components

### *Alexandra Bridge/rue Laurier Node View from northeast*

*This node was developed during construction of Confederation Boulevard and in association with the development of the Museum of Civilization. It features an effective landscaped space, but still requires a major commemoration to give it visual presence and a sense of meaning.*



### *Laurier/Taché Node View from northwest*

*This location was redeveloped in the 1990s. The development of a meaningful sense of spatial definition was constrained by the existing topography and industrial infrastructure. The location could be developed into an effective node when that infrastructure is removed or relocated and when a site for a significant commemoration is required.*







*Wellington/Portage Node  
View from southwest*

*This location is a major intersection developed in the 1970s when Portage Bridge was constructed. It bears no resemblance to a Confederation Boulevard node, and will require major reconstruction to achieve that ambition.*



*Rideau Hall Node  
View from northeast*

*This location is the northern terminus of Confederation Boulevard and was redeveloped in the early 2000s. The new roundabout enjoys some visual prominence, but lacks a significant marker or commemoration.*

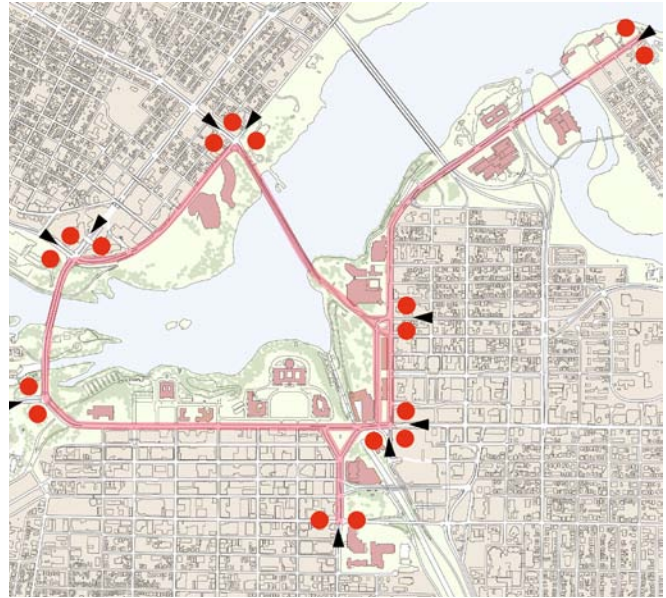


## 2.8 The Gateways

The major points of entry to Confederation Boulevard are developed as gateways.

In many cases, the nodes themselves serve as gateways by virtue of their visual strength and presence. The Peacekeeping Monument, for instance, is a powerful composition that clearly indicates a sense of transition from the urban fabric to the Boulevard. At nodes, this sense of entry can be reinforced through the use of flags, banners and lighting.

Gateway development at this point is uneven, and will only be improved as the nodes themselves are developed. The north end of Sussex Drive is a good example of a point of entry being identified through the use of Threshold Lights and special paving. The foot of Elgin Street at Laurier is similarly identified by Threshold Lights, but these tend to be lost among a maze of City street lights, traffic signals and signs.



Gateways



Sussex Drive: Threshold Lights forming the gateway at the northern terminus



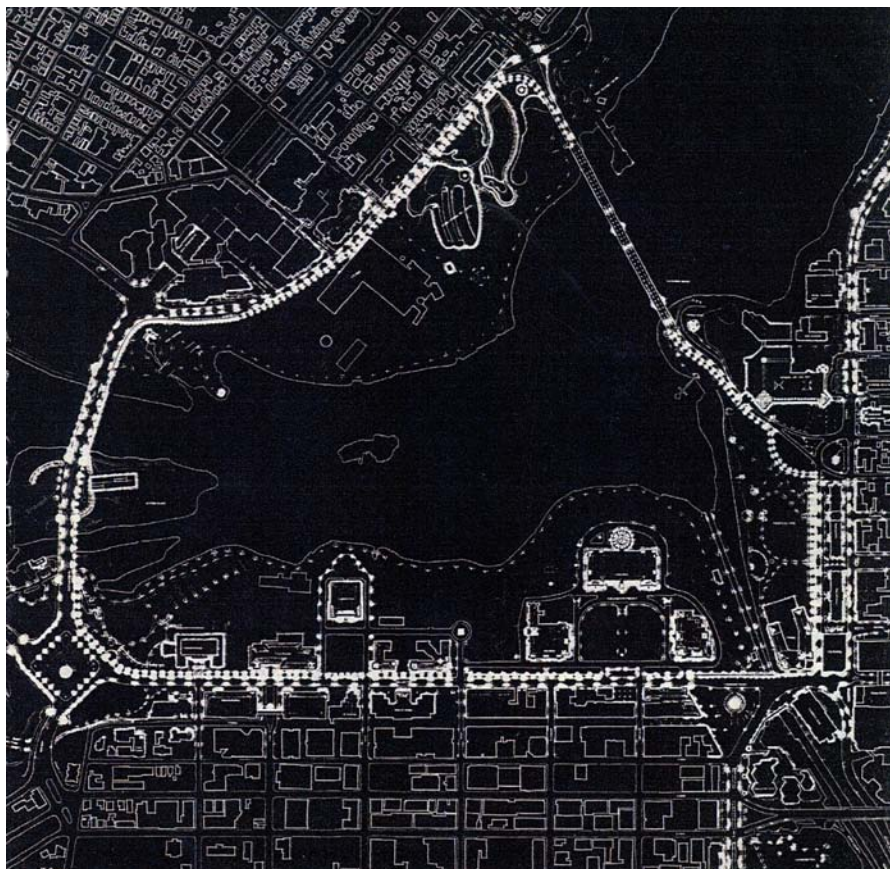
## 2.9 Lighting

A lighting system for Confederation Boulevard has been developed, which reveals in striking fashion the structure and components of the Boulevard after dark. Lighting has been used to strengthen the sense of a linking ring punctuated by nodes and entry points. There is a sufficient degree of uniformity among light fixtures and how they are used to create a cohesive image, while reflecting the diversity of the parts that make up the Boulevard. The system has been designed to enhance the experience of pedestrians and motorists alike while ensuring the safety of all users.

A key feature of the system is the globed fixture, which creates highly visible ribbons of light along the connecting streets and sidewalks.

Key features include:

- A double row of pedestrian fixtures define the Grand Esplanade as in a “string of pearls”.
- Carriageways are illuminated with elevated globe fixtures extending over the roadway on curved arms. The fixtures, located on both sides, arch over the carriageway to define it as a positive space.
- Nodes receive higher levels of illumination to reinforce their importance as turning and entry points. The lighting design for each node is unique, reflecting the specific site circumstances found on the Boulevard.
- Buildings, monuments, bridges and other landmarks along the Boulevard are illuminated according to their roles and symbolic importance. Special emphasis is given to the federal buildings on the Crown side to distinguish them from the more subdued background created by the Town Side building fabric.



*Confederation Boulevard lighting concept (Gabriel, 1985)*



*St. Patrick Street: the 'String of Pearls'*





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## PART 3 - KEY PRINCIPLES

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*Introduction*  
*A Memorable Image*  
*A Vibrant Public Place*  
*Pedestrians First*  
*Universal Accessibility*  
*Sustainability*

This section sets out key principles that embody the fundamental goals and objectives for Confederation Boulevard. The principles cover development, maintenance and management, and will guide the stewards of Confederation Boulevard well into the future.

## Introduction

Now that Confederation Boulevard has been largely implemented, the planning emphasis shall shift to protection, incremental improvement, maintenance and management.

The 1983 report ***Ceremonial Routes*** sets out a series of principles and strategies covering the basic structure and form of Confederation Boulevard. These have largely been respected in the design of the Boulevard, and will continue to form the basic planning foundation. The following are key principles that should govern future decisions and actions. They build upon the foundation principles, but reflect current issues and priorities that are emerging as the Boulevard continues to mature and take on a life of its own.

A major challenge in the future will be coordinating the efforts of the different agencies that have an interest in the quality, character and use of the Boulevard. These agencies include the National Capital Commission, Public Works and Government Services Canada, the Cities of Ottawa and Gatineau, OC Transpo and STO, and various service and utility companies. It is hoped that the principles in this guideline will be embraced by all and will direct future actions.



*A consistent vocabulary of materials and components...*



## 3.1 A Memorable Image

***Confederation Boulevard projects an image that is dignified, unique and lasting, and that is reflective of Canadian values, heritage and achievements.***

### 3.1.1 Cohesive and Distinct Image

Confederation Boulevard should be a unique and special place that is distinct from the two cities it spans.

Confederation Boulevard must have a unique and distinctive character if it is to successfully mediate between Town and Crown, accommodate the Nation's most significant commemorations and facilitate the ceremonial progression of dignitaries. Through its materials, furnishings, decorations and management, Confederation Boulevard must continue to feel like a special place that is not only different from other places in the National Capital, but also unique in the country and beyond.

Any inclination to introduce elements that are commonly found elsewhere in the two cities and the temptation to re-use Boulevard materials and components in other parts of the National Capital should be resisted.

### 3.1.2 Noble Materials

Materials used on Confederation Boulevard should reflect a sense of permanence and enduring stability.

Granite, steel, aluminum, concrete and fine woods are tough, long-lasting materials that should form the basic palette of Confederation Boulevard. These materials are perhaps the only materials that can stand beside the Parliament Buildings and other national institutions along the Boulevard, which themselves radiate a strong sense of permanence and stability.

Once the basic armature of Confederation Boulevard is confirmed, other materials can be introduced if used with restraint. Fabrics, glass, plastics, rubber and laminates can be used for accent and sparkle, for temporary or seasonal displays, and to meet UA objectives.

### 3.1.3 National Symbols

Confederation Boulevard should reinforce the physical symbols that give meaning to the National Capital.

A major purpose of Confederation Boulevard is to present and celebrate the National Symbols. These include the Parliament Buildings, Supreme Court and



...A memorable image

### 3. Key Principles

the National War Memorial among others, and represent political and cultural values of the highest national importance.

Confederation Boulevard provides a setting for the National Symbols that must be commensurate in quality and character. The Boulevard should always be refined, dignified and respectful; it should never become diluted; it should never trivialize; and it should never fall into disrepair.

#### **3.1.4 An Icon of Canada**

Confederation Boulevard should always be identifiable as a place that is singularly Canadian.

As an important component of the National Capital setting, Confederation Boulevard should reflect values and project images that represent the Canadian reality.

Materials and furnishings should be sourced in Canada, and designed and fabricated by Canadians. Granite, concrete, wood and steel are not unique to Canada, of course, but are founding materials used across the Nation and familiar to most Canadians.

Commemorations and public art on Confederation Boulevard should similarly represent and express the Canadian experience. The subjects should reflect national interests and aspirations, and materials should similarly be recognizable as Canadian.

#### **3.1.5 Evolving Aesthetic Values**

The design of Confederation Boulevard may, over time, evolve to reflect contemporary styles and motifs.

Quite in addition to the aesthetic changes brought about by new technologies, the visual character of Con-

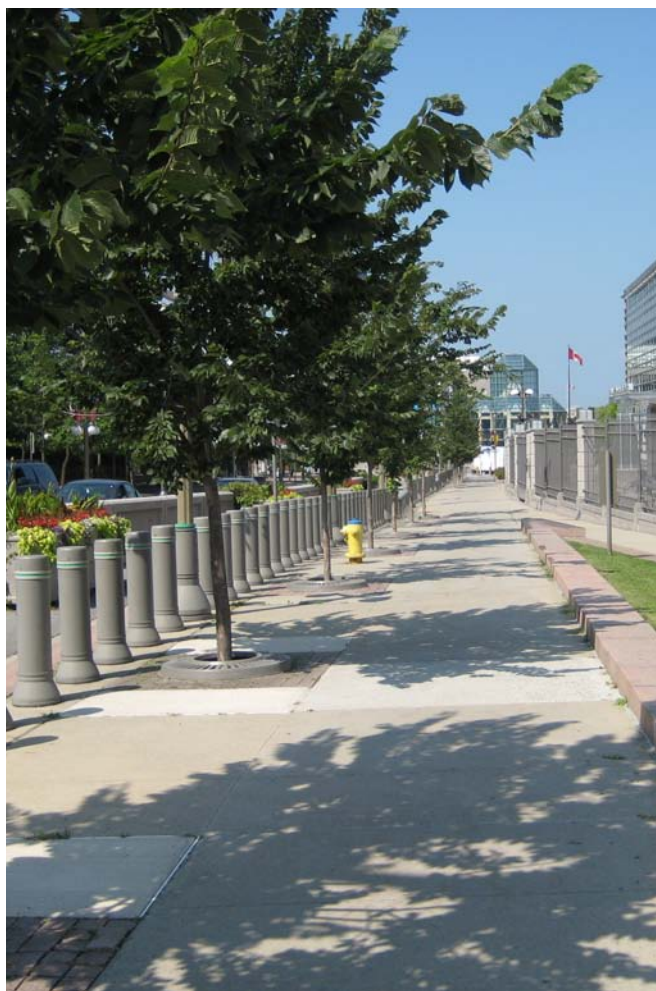


*Contemporary styles and motifs on Confederation Boulevard*



Confederation Boulevard will most likely evolve over time. While it should always project a dignified image fitting for the setting of the National Symbols, it is not a heritage setting in itself that should be preserved in perpetuity. Through a natural (but deliberate and intentional) process, Confederation Boulevard should evolve aesthetically to establish its identity as a vibrant, exciting and current place that is very much of its time.

The current design of Confederation Boulevard, as expressed through the lights, benches and other furnishings, is traditional in approach. The underlying philosophy was that to serve as the setting for the National Symbols the Esplanade should itself reflect the historical character of those symbols, and Parliament Hill in particular. This philosophy breaks down, however, since the Esplanade also serves as the setting for the National Gallery of Art and the Canadian Museum of Civilization, both of which are very modern buildings.



*Bollards and barriers in front of the American Embassy*

Changes to the aesthetic expression of Confederation Boulevard should be implemented with great care. The goal should always be to respect and support the important imagery and symbols of the National Capital, both historical and modern. The Boulevard can be modern, but it should never be flashy, and it should never capitulate to momentary fads and passing trends.

### **3.1.6 A High Level of Maintenance**

As a Canadian icon, Confederation Boulevard should always be maintained in a high state of repair. It is a showcase of Canadian design and craftsmanship, and should remain as a point of pride for all of us. The investment that went into constructing the Boulevard should now be matched by a similar investment in keeping it looking as good as it can.

Even with high quality materials, the Boulevard will require attention to ensure that pavements, plantings, furnishings and other components retain their lustre and continue to fulfil the goals set out for the Boulevard. To achieve this, a coordinated program of daily and yearly maintenance should be established that reflects a high degree of collaboration among the various jurisdictions that share responsibility for Confederation Boulevard.

### **3.1.7 Image and Security**

Confederation Boulevard should offer a high degree of personal safety and incorporate new institutional security measures without compromising its character.

The stewards of Confederation Boulevard should seek to minimize the impact of institutional security measures. Existing physical examples that affect Confederation Boulevard include the barricades adjacent to the American Embassy and the concrete barriers blocking the Queen's Gate at Parliament Hill. Other more severe security measures that may impact the Boulevard are currently being considered for Parliament Hill.

While important, new security features and strategies should be planned with full regard for the public nature of Confederation Boulevard and the important images of dignity, pride and openness that it should project.

## 3.2 A Vibrant Public Place

***Confederation Boulevard is a vibrant public place that presents Canadians with a range of opportunities for enhanced community, intellectual and emotional experience.***

### 3.2.1 A Social Space

Notwithstanding that the Boulevard is the setting of many important and dignified National Symbols, it is also a public place that accommodates activities common to all public places. Strolling, walking to work, cycling, chatting with friends, enjoying the scenery, sitting in the sun, playing on the grass are normal activities that should be encouraged simply because the Boulevard is a public place, and one of the most important in the Nation. An active public place is also a safe place.

These activities can be encouraged by ensuring that the Boulevard is safe, attractive, interesting and accessible to all. It is certainly interesting with many important destinations and landmarks along its length, but it should also offer a fine-grain layer of interest in the quality of its design and materials.

Public activity on the Boulevard can also be encouraged through the provision of high-quality amenities. Benches, waste receptacles and wayfinding signage are basic amenities that should be available at regular intervals along the route. Wayfinding signage and interpretive panels put people at ease and increase their level of interest.

### 3.2.2 A Dynamic Place

Confederation Boulevard should provide fresh experiences through a broad range of seasonal and temporary displays, activities and events.

While Confederation Boulevard will present a timeless expression on one level, it should also be constantly changing on another.

Seasonal change in the National Capital is both dramatic and inevitable. Confederation Boulevard should embrace and celebrate seasonal change through the use of vegetation that offers a dramatic display in all seasons: blossoms in the spring, flowers in the summer, leaves in the fall, and sculptural branching forms and colourful twigs in the winter.



*Confederation Boulevard: A very public place*



The various temporary displays mounted by the NCC are also an important means of maintaining the dynamic quality of the Boulevard experience. The well-established banner and flag programs offer colour and interest throughout the year and can signal special events such as the arrival of important dignitaries. Floral and decorative lighting displays animate the Boulevard and further enhance the delights of seasonal change.

### 3.2.3 *Life and Vibrancy*

Confederation Boulevard should encourage a more lively public environment through site animation programs, partnerships with private sector animation programs, more food vendors and increased seating areas.

Summer festivals in the Capital should encourage more informal programming and street animation along Confederation Boulevard through, for example, satellite stages that help promote the Capital Experience. Closing streets to hold festivals should be encouraged, as is done in other world class cities.

People attract people. Therefore, better site amenities in strategic locations along Confederation Boulevard would greatly improve the liveability and sense of place.

### 3.2.4 *Commemoration and Public Art*

Confederation Boulevard shall remain the venue for commemorations and public art of national significance.

Because of its inherent quality and proximity to key national institutions, Confederation Boulevard is a most sought after location for commemorations and public art. The limited number of prime sites along the Boulevard should, however, be reserved for installations that reflect events, values and aspirations of national significance. Key examples include the National War Memorial and the National Peacekeeping Monument.

The NCC's **Capital Commemoration Strategy** (2006) identifies other potential sites along and in the vicinity of Confederation Boulevard.



*Flags and Banners: A fresh experience*



*Canadian public art: Maurice Richard*

### 3.3 Pedestrians First

***Confederation Boulevard gives first priority to the comfort, safety and enjoyment of pedestrians, and then to the accommodation of cyclists, public transit and other vehicles.***

Confederation Boulevard is a series of important transportation routes serving the downtowns of Ottawa and Gatineau. Of particular importance are the two river crossings, which serve as vital multi-modal links. These routes serve all forms of transport from pedestrians and cyclists through to cars, buses and trucks. Nevertheless, priorities should be established that recognize symbolic, social and cultural importance of Confederation Boulevard.

#### 3.3.1 Pedestrians

On Confederation Boulevard, the highest priority should be given to pedestrian traffic. Pedestrians, rather than those moving through in vehicles, are those who experience the Boulevard most closely. It is pedestrians who are engaged in events and activities; it is pedestrians who are enjoying views of river landscapes; it is pedestrians who are appreciating history and heritage; it is pedestrians who are learning about the National Capital and the values that define a nation.

Quite apart from the particular purpose and character of the Boulevard, the accommodation of pedestrians is now assuming a much greater importance in most municipalities. Over-reliance on the personal vehicle has led to an unbalanced relationship between pedestrians and vehicles that many now recognize as being contrary to good city-building. The NCC together with the Cities of Ottawa and Gatineau now advocates greater priority for pedestrians on city streets.

Providing for pedestrians on Confederation Boulevard has underpinned the design of the Boulevard from the outset, and should remain a central objective. All sidewalks should be generous and unobstructed and the Esplanade, in particular, should be maintained as a broad promenade extending uninterrupted around the Linking Ring. Street crossings should occur frequently and should be short and signaled where possible. Suitable amenities - benches, shelters, drinking fountains - should be provided to meet peoples' needs and further encourage pedestrian activity.

As a public place, Confederation Boulevard should offer all users a high degree of personal safety. Future design and management should observe the principles promoted through Crime Prevention Through Environmental Design (CPTED). CPTED's key principles concern visibility, surveillance, access control, means of escape, levels of activity and maintenance. Aspects of Confederation Boulevard that should continue to



*Pedestrian priority on Confederation Boulevard*



receive attention from this perspective include lighting, planting design and programming.

### 3.3.2 Cyclists

As a way of promoting active mobility to and from the downtown core, cyclists should be encouraged to use Confederation Boulevard. As in most municipalities, cycling is becoming a much more popular means of both recreational and commuting transit in the National Capital, and usage is becoming almost year-round.

Cyclists should be provided with appropriate on-street facilities to minimize conflicts with pedestrians on the Esplanade and other sidewalks. These should include designated bike lanes segregated from vehicles where possible. Already illegal, bicycle riding should be actively discouraged on the Grand Esplanade and other sidewalks.

A bike-share program is now being implemented in the Capital. Bike-share rental stations should be located near cycling facilities to encourage cycling connections with the capital pathways and municipal cycling networks. Stations should be located on the roadway, where possible and with the necessary protection, or within parking lots or building entrances, to encourage cyclists to use on-road facilities rather than sidewalks.

### 3.3.3 Vehicular Traffic

As part of a wider street network, Confederation Boulevard will continue to accommodate relatively high volumes of vehicular traffic. In fact, the Boulevard plays important roles as the ceremonial route taken by dignitaries and as a preferred route taken by many visitors to the Capital.

Some priority should be given to providing convenient and efficient public transit. Not only does public transit offer an efficient way for visitors to reach the Boulevard, but the presence of public transit also generates the pedestrian traffic that animates it and keeps it safe.

Lower priority should be given to accommodating private vehicles and trucks in particular. While commuter traffic will never be eliminated from Confederation Boulevard, continuing efforts should be made to encourage drivers to switch to other routes or other transit modes.



*Safer cyclist accommodation is required*



*Convenient and efficient public transit*

### 3.4 Universal Accessibility

**Confederation Boulevard is accessible to all persons, and shall extend accessibility to those beyond the National Capital through a range of communication media.**

#### 3.4.1 Physical Accessibility

Confederation Boulevard should strive to be accessible to all people.

As Canada's pre-eminent public place, Confederation Boulevard should be accessible to all people including those with physical, mental or sensorial disabilities. The primary parts of the Boulevard are, in general, physically accessible to all people. There are very few locations where physical barriers exist.

However, one area where improvements could be made are intersections. Many of the Boulevard streets are major arteries carrying high volumes of traffic. Intersections should be carefully designed and managed to ensure that people with mobility limitations have sufficient time to make the crossing safely. In areas heavily populated by pedestrians, it should be accepted that levels of service for vehicular traffic will be reduced in order to achieve this objective.

#### 3.4.2 Long-distance Accessibility

Confederation Boulevard should also be regularly accessible to those who may live in distant parts of the country. Electronic communication techniques have evolved to a point now that almost everybody is able to access vast amounts of information including detailed images of remote locations. These capabilities should be exploited by the NCC as another way of improving access to Confederation Boulevard. Among others, these should include web sites that report on daily activities and developments, virtual tours, educational and outreach materials, and real-time webcams that present ongoing events.

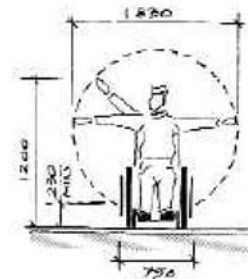


Figure 5.2  
Person in a Wheelchair

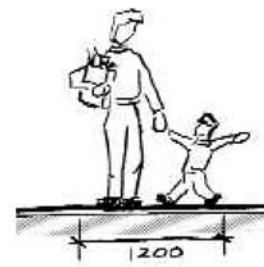


Figure 5.3  
Person Accompanied by  
Child or Guide Dog

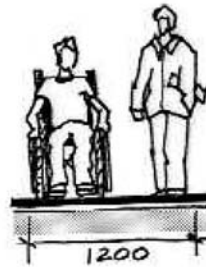


Figure 5.5  
Pedestrian and Wheelchair  
User Side-by-Side

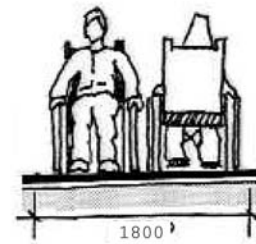


Figure 5.6  
Two Wheelchair Users  
Side-by-Side

**Best Practices to the Accessible Design of the National Capital Commission's Outdoor Spaces, NCC 2010**

The screenshot shows a webpage for Confederation Boulevard. At the top, there is a banner for the 'Launch of the 2012 National Student Banner Contest' with a photo of colorful banners. Below this is a 'Confederation Boulevard' section with social media icons and a 'Where is Confederation Boulevard?' section. The 'Where is...' section lists three parts: Central loop, North-eastern section, and Southern section. To the right, there are 'Maps and Publications' and 'News' sections. At the bottom, there is a 'The Sights of Confederation Boulevard' section with a list of attractions.

NCC website: <http://www.canadascapital.gc.ca/places-to-visit/confederation-boulevard>



## 3.5 Sustainability

***Confederation Boulevard demonstrates leadership in environmental sustainability and stewardship.***

### 3.5.1 Sustainability

The design and maintenance of Confederation Boulevard shall incorporate best practices in sustainability.

While Confederation Boulevard was developed just as the philosophy of sustainability was emerging, future work should seek to implement these principles wherever possible. Quite apart from the intrinsic benefits of implementing a sustainable approach, Confederation Boulevard should, as one of Canada's premier public places, be seen to be in the forefront of environmental responsibility.

Going forward, Confederation Boulevard should demonstrate best sustainable practices wherever possible:

- locally sourced and high quality building materials and products;
- indigenous plants;
- reduced irrigation;
- on-site storm water management;
- recycled and recyclable materials;
- alternate energy sources;
- waste reduction;
- energy conservation.

The NCC is currently adopting revisions to its Procurement Policy that include more rigorous environmental guidelines and requirements. All future goods and services contracts and Request for Proposals will require that these guidelines are observed.

Similarly, the NCC is developing strategies for tracking and managing its carbon footprint. In future, staff and contractors will be required to provide the Environmental Management and Protection Division with energy data related to fossil fuel consumption (mileage, gas consumption, heating, and electricity usage). These policies are also being applied to major NCC events such as the Christmas Lights and Canada Day to measure impact and establish offsetting measures.

### 3.5.2 Evolving Technology

The ongoing development of Confederation Boulevard should embrace new, proven technologies, interactive media and construction techniques as they arise.

New construction and energy-related technologies are being developed that can make Confederation Boulevard more sustainable, innovative and perhaps easier to maintain. Moreover, as one of Canada's premier public projects, Confederation Boulevard should embrace the advantages of new technologies and techniques.

For example, there are three areas where current technological developments should continue to be explored as a matter of priority. New tree-planting techniques - including CU Structural Soil and Soil Cells - are being tested in the National Capital and other jurisdictions to improve the conditions necessary to promote vigorous tree growth, especially in paved areas. Similarly, there is much work being undertaken to adapt highly energy-efficient LED light technology to outdoor street and security lighting.

Thirdly, digital communication and interactive Wi-Fi media are changing the way we do our daily business.



*Soil Cells: Improved tree planting technology*

### 3. Key Principles

Adopting new technologies will, in some cases, have aesthetic implications for Confederation Boulevard. While some technological advances will be invisible because they are underground or “behind the scene”, others will alter the appearance of the Boulevard to a greater or lesser extent. LED lighting is a good example of the latter in that the quality of light may be slightly different, and the fixture may be altered in appearance.

Aesthetic changes that accompany new technologies should be carefully assessed to ensure compliance with the overall character of the Boulevard.

The expression of the Boulevard should always be unique, and should always remain consistent along its length. Changes to the aesthetic image of the Boulevard should, therefore, be implemented as broad-based programs associated with the life cycle management and upgrading of furnishings and other components.

#### **3.5.3 Partners in Stewardship**

The management of Confederation Boulevard should reflect a spirit of cooperation and mutual support among partners.

The development, maintenance and operation of Confederation Boulevard require the close cooperation of many partners including the National Capital Commission, Public Works and Government Services Canada, the Cities of Ottawa and Gatineau, and a number of both private and public landowners.

Stewardship is further complicated by the complex structure of individual partners. Most consist of several departments carrying different responsibilities for various aspects of the Boulevard.

For Confederation Boulevard to establish and retain a singular image, and for it to operate in ways that are consistent with its role as the setting for the National Symbols, all of these agencies must share a common vision of the Boulevard.



*New technology may promote a shift in image*



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## PART 4 - DESIGN VOCABULARY AND MATERIALS

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*A Palette of Fundamental Materials*  
*A Family of Furnishings*  
*A Family of Signage and Graphics*  
*Trees on the Boulevard*  
*Temporary Installations*  
*Municipal and Other Features*

This section describes the physical materials and furnishings of Confederation Boulevard. While the details may shift in response to pressures we cannot foresee, the general patterns define a basic armature that should remain constant.

## Introduction

The character of Confederation Boulevard is distinct and identifiable to the extent that materials, furnishings and other features are themselves unique and consistently applied.

This has been no small challenge. First, the development of the Boulevard has spanned more than two decades and employed the services of many different managers, designers and contractors. To maintain a consistent line of thinking over that period of time and with such diverse individuals has required considerable discipline and perseverance. Second, Confederation Boulevard is made up of regular municipal streets with all the functional and legislative requirements that so strongly determine the character of city streets everywhere. To develop a cohesive and distinctive character for the Boulevard streets has required a steadfast vision coupled with a high degree of collaboration between the NCC and municipal and other agencies.

Over the years, the National Capital Commission has worked hard to develop the palette of components and establish the consistent means of application necessary to realize the goal of Confederation Boulevard. While changes over time may be necessary for a variety of reasons, the quality and general consistency of the palette should be maintained as a matter of high priority.



*Confederation Boulevard: distinct and identifiable*



## 4.1 A Palette of Fundamental Materials

The basic building blocks of Confederation Boulevard include pavements, curbs, steps, retaining walls, planters, railings and guards. All of these components are constructed from materials that are high quality, yet durable and economical. Materials and components should always be replaced by materials that are equal to or higher quality.

### 4.1.1 Finished Granite

Granite is a signature material that identifies Confederation Boulevard. The colour, richness and durability of the granite reflect well the intended purposes and character of the Boulevard. Typical applications include curbs and decorative borders.

On the Esplanade, red granite is used for curbs and borders to reinforce its special role and identity. Granite curbs are flame-finished on tops and verticals with a 25 mm chamfer. Rear granite curbs and borders are provided where the Esplanade is adjacent to lawns.

Along Elgin Street and Sussex Drive, granite curbs are provided (without granite borders). They are flame-finished on tops with split face finish on the vertical surface.

Granite setts are used to define the tree pits. (See page 65.)



*Granite curbs and borders along the Esplanade*



*Granite curb and border on Mackenzie Avenue*

**4.1.2 Granite Cobbles**

Granite cobbles are used on the very broad Elgin Street sidewalks and in the median. They lend a special identity to this important ceremonial approach and provide a permeable paving solution that benefits the trees along the curb.

The cobbles used on Confederation Boulevard are recycled units that were originally brought over from Europe as ballast. In North America, they were first used on city streets in Montreal and then were acquired by the NCC. Much of the character of these cobbles comes from their inherent variations. They vary slightly in size and shape, but are generally 100 mm x 100 mm x 200 mm. The cobbles also vary in colour, generally showing a range of shades of brown.



*Granite cobbles in the Elgin Street median*



*Granite cobbles under the trees on Elgin Street*



4.1.3 Concrete Unit Paving

Concrete unit paving, called “Megapaver” by Permacon, is the primary pavement along the Esplanade. It is laid in a custom-designed random pattern except at drive-ways where a staked bond pattern is used.

The colour of the unit pavers is “Caledonia Red”, a custom colour developed for the Boulevard. The surface is natural; edges have a 10 mm chamfer. The pavers come in a range of sizes that are carefully organized to approximate a random pattern.

The concrete unit paving used on the Boulevard has proven to be a sturdy and reliable material requiring very little maintenance. The granite unit paving, which was used in the first phases along the St. Patrick approach to Alexandra Bridge and in front of the Museum of Civilization, is now being replaced with concrete unit paving.

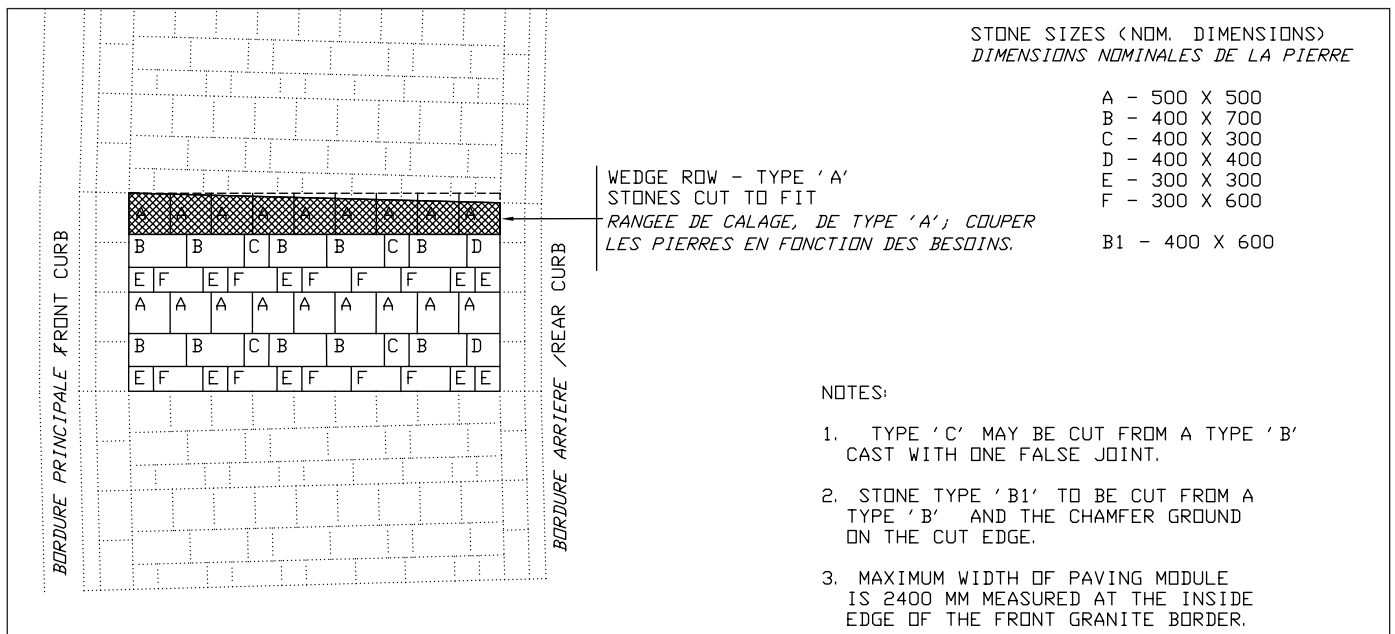
Megapaver was produced by Permacon. However, the line is currently being phased out and will only be temporarily produced for projects where it is already in use. The NCC is pre-ordering the material to make sure that sufficient quantities are available for future phases and repairs.



Concrete unit paving during construction



Concrete unit paving on Wellington Street



Typical concrete unit paving pattern

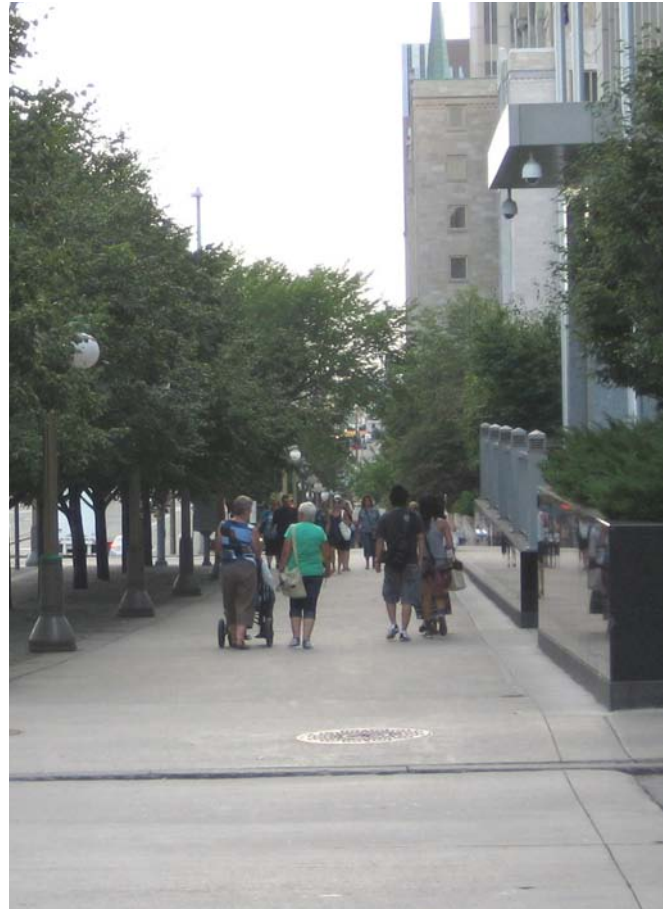
#### 4.1.4 Concrete Paving

If carefully placed and finished, concrete is a fine material that is both adaptable and economical.

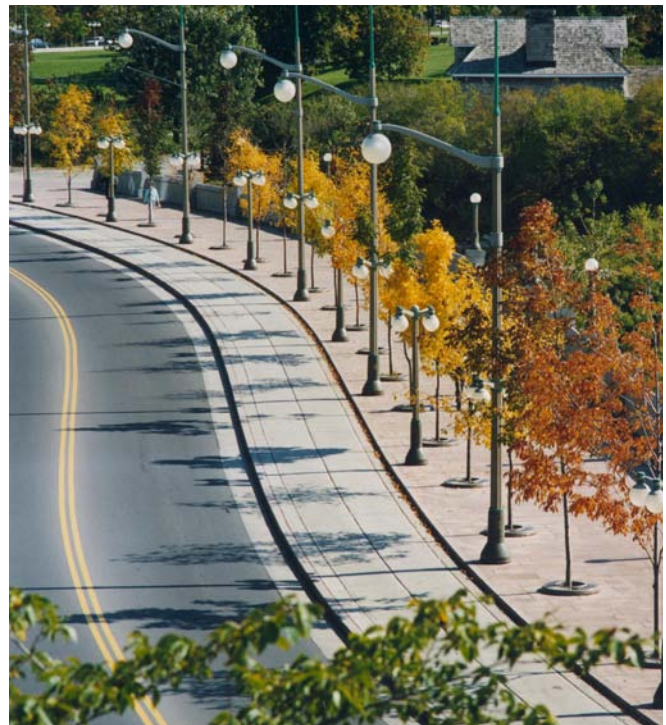
Concrete paving is used extensively on Confederation Boulevard as the predominant sidewalk and crosswalk paving for the Town side as well as Elgin Street and Sussex Drive north.

Poured concrete is also used on the Esplanade to demarcate two-way bicycle lanes. These are located along the south approach to Alexandra Bridge and across Portage Bridge.

Concrete is standard Portland cement with broomed finish. Wellington Street concrete has granite aggregate to give a slight reddish tint. Concrete elsewhere contains standard limestone aggregates.



*Poured-in-place concrete paving on Elgin Street*



*Concrete bicycle lanes on St. Patrick Street*



**4.1.5 Precast Concrete**

Precast concrete is used extensively on Confederation Boulevard because of the finer detail and more consistent finish that can be provided. It is typically used for steps, copings, bridge markers, railing posts and other components that enjoy high visibility.

Precast concrete for Confederation Boulevard includes granite aggregates to obtain a slight reddish tint. All exposed surfaces receive a medium sandblast finish to reveal the aggregate.



*Precast concrete stair during Installation*

**4.1.6 Poured-in-place Concrete**

Poured-in-place concrete is used on the Boulevard for planter curbs, retaining walls and other components where precast concrete is either not warranted or not technically feasible.

Precast and poured-in-place concrete are often used together. The photo opposite shows precast steps in the foreground, a poured-in-place wall with a precast coping and a poured-in-place planter curb beyond.

Poured-in-place concrete is designed to match precast and includes granite aggregates to obtain a slight reddish tint. Visible poured-in-place concrete components receive a medium sandblast finish.



*A typical combination of poured-in-place and precast concrete*

**4.1.7 Metalwork**

Metal fabrications are used on the Boulevard in a variety of applications including bridge railings, handrails and fences. All installations are custom designed and fabricated to suit specific site circumstances while following the general Boulevard design theme.

Metal is welded galvanized steel; the colour is standard CB grey. Finishes are factory or site applied depending upon specific design and site circumstances. Metalwork is brought to the site either whole or in sections. Installation and erection should rely upon bolted connections rather than site welding to maintain control over the final finish



*Metal bridge railing: Portage Bridge*



*Metal fence: rue Laurier*



## 4.2 A Family of Furnishings

Over the years, the NCC has worked with designers and manufacturers to develop lights, bollards, benches, and other furnishings that are appropriate and unique to the Boulevard. It is this family of furnishings that contribute so significantly to the distinctive character and visual cohesion of Confederation Boulevard.

### 4.2.1 Lights

Designed in the mid-1990s, a family of light fixtures both identifies and unifies Confederation Boulevard. They have been very successful, both during the day and at night. When asked in surveys, most people cited the lights as among the most distinctive and memorable features of the Boulevard.

Key features include globe luminaires, curved fixture arms and fluted poles. Fixtures are CB grey with green accents. Metal Halide (MH) luminaries are used in the street lights for brightness and enhanced colour rendition; Compact Fluorescent Lamps (CFL) are used in the pedestrian lights for colour warmth and economy. The globe fixtures are fitted with reflectors to reduce upright and glare.

The key Boulevard fixtures include:

**Street Light (SL)** - standard street light used throughout the Boulevard;

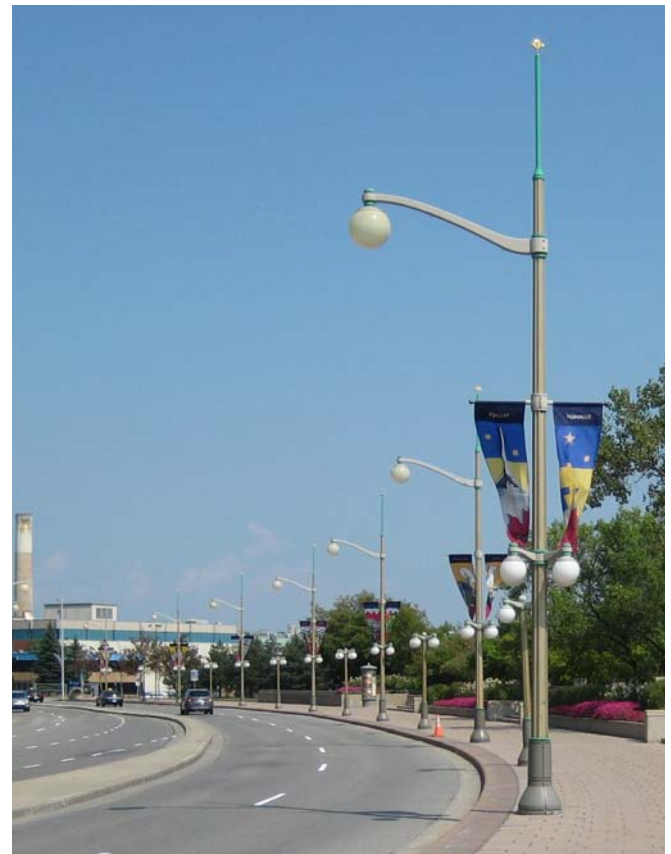
**Street/pedestrian Light (SPL)** - enhanced street light with pedestrian globes; used along the Esplanade;

**Pedestrian Light (PL)** - standard pedestrian fixture with four globes; alternates with the SL;

**Threshold Light (TL)** - Multi-globed street light used at the gateways to the Boulevard.

There are over 800 lights on the Boulevard, most of which are SL/SPLs and PLs. A series of other fixtures called CR (variations on the Pedestrian Light) are used on bridges and retaining walls, and throughout the parks adjoining Confederation Boulevard.

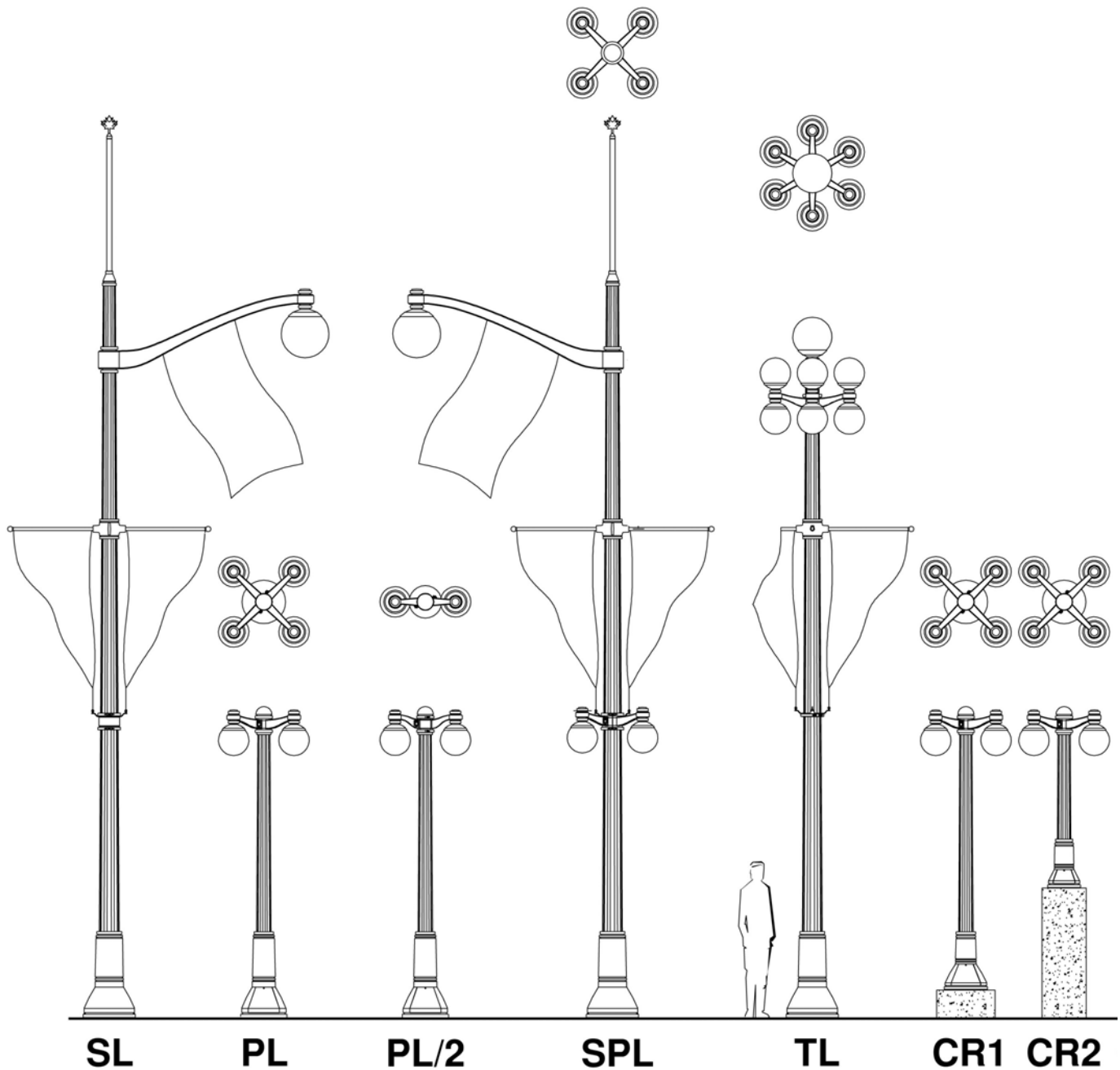
Illustrations of the Confederation Boulevard family of light fixtures are presented on the next page.



*A row of alternating SPL's and PL's: rue Laurier*

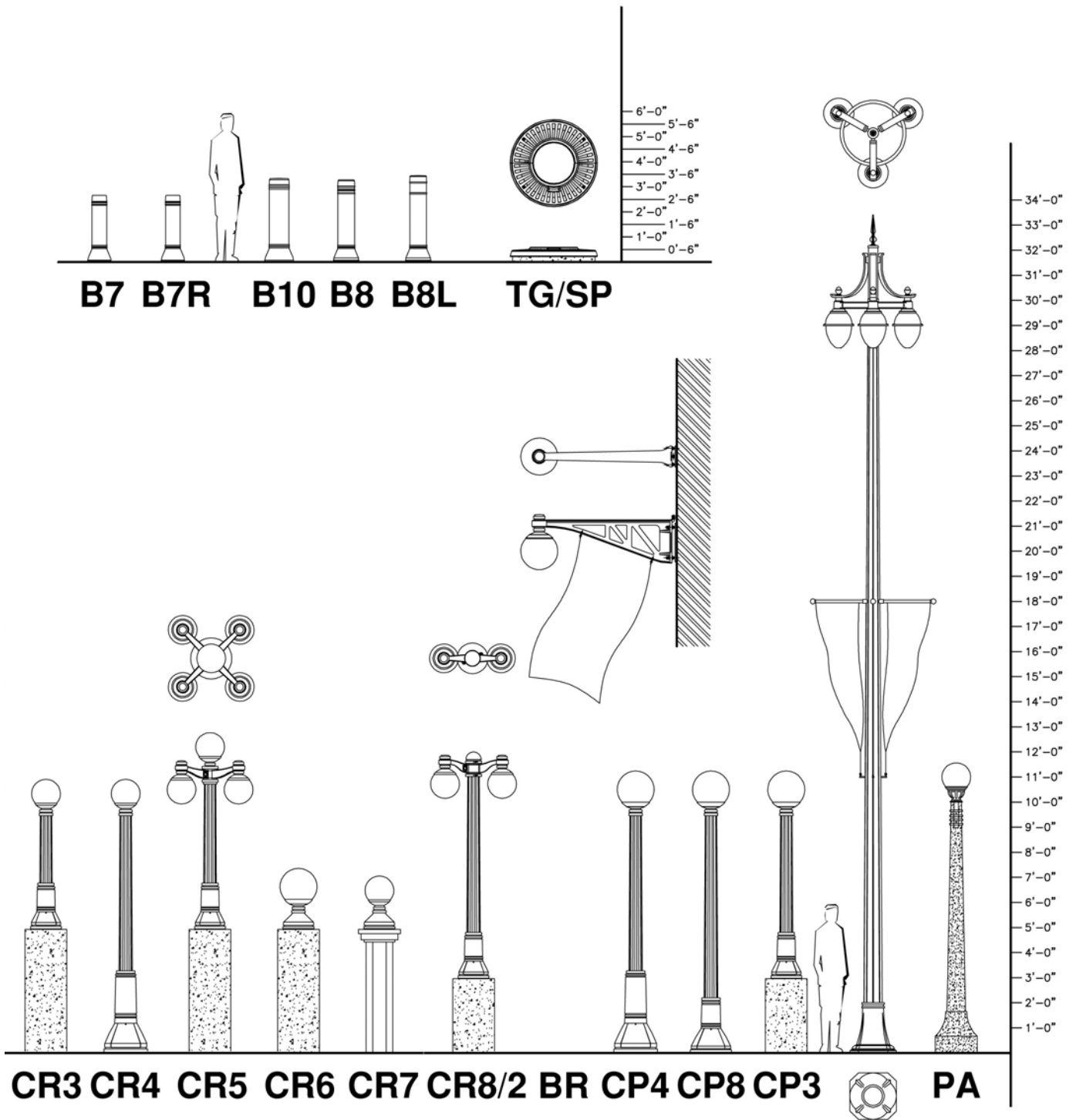


*TL and SPL lights: Rideau Hall*



*A family of light fixtures and bollards was developed specifically for Confederation Boulevard to establish a unique and unified visual character. Most of the lights and furnishings have been installed on the Boulevard. Some of these including the SL, SPL and PL are extensively used and form the backbone of the Boulevard image. Those fixtures that have not yet been used can be assembled, if required, from the CB "parts kit".*





### 4.2.2 Bollards

A family of metal bollards have been installed on the Boulevard to control vehicular access and supplement institutional security. Specially reinforced bollards (in combination with continuous concrete barriers) have been installed along the Mackenzie/Sussex street front-ages in front of the American Embassy to provide perimeter security. Illuminated bollards have been installed in some of the parks adjacent to the Boulevard.

Bollards are welded steel painted CB grey with green trim. See the chart on the previous page.

### 4.2.3 Benches

Benches are an important amenity on the Boulevard, offering places to rest and watch, and expressing a sense of welcome and ease. Confederation Boulevard benches are custom fabricated for the NCC in wood and steel. The wood is ash with a clear finish and the steel is painted CB grey.

Benches are located in places where people naturally linger to watch other people, enjoy attractive scenery or pause to rest. In order to provide people with a sense of security, benches should be located at the protected edges of public spaces, or associated with planters or other elements. Where space permits, benches should be installed in groups of two or more with space between to accommodate persons in wheelchairs.

Benches should be found at regular intervals along the Boulevard to provide frequent rest stops without blocking the path of travel. Furniture plans and event programming should be coordinated to establish an appropriate balance between these two important requirements.

Benches are typically bolted to the concrete unit paving. Some benches in restricted locations are temporarily removed in winter to facilitate snow clearing.

In 2010, there were about 145 benches along the Boulevard.



*Bollards: American Embassy*



*Benches at the edges of public spaces*



### 4.2.4 Waste Receptacles

Waste receptacles are located at key gathering spots including rest areas, intersections and bus stops. They are decorative welded steel painted CB grey.

Consideration should be given to introducing a coordinated waste and recycling program.

### 4.2.5 Concrete Planters

Low, square planters are used throughout the Boulevard for seasonal floral displays in spring and summer. Being fairly easily transported, planters can be rearranged to suit changing requirements or large events. They are all removed and stored in the winter.

Planters are moulded from precast, untinted concrete to a custom NCC design.

### 4.2.6 Bike Parking

Bike parking racks are provided at a few locations along Confederation Boulevard. These are welded tubular steel fabrications painted CB grey.

Many more bike racks should be provided on the Boulevard to service a growing need and discourage people from locking bikes to trees, poles and other furnishings. Consideration should be given to providing single or double bike racks located more frequently along the Boulevard rather than a few multi-bike racks. Bike racks should be associated with building entrances and other well-used destinations.

A public bike-share system is being implemented for the National Capital that features relatively large bike parking/rental stations. Some of these stations should be located along the Boulevard and near important destinations, but they should not be located on the Esplanade or in areas designated for pedestrians. The location of bike-share stations must guide cyclists to ride on the road rather than on sidewalks.



Waste receptacle: Mackenzie Avenue



Concrete planters associated with benches



Bike racks on Sussex Drive

### 4.2.7 Drinking Fountains

Drinking fountains are provided as a public convenience, usually in association with benches and other amenities. Drinking fountains are precast concrete with stainless steel fittings. There are two different fountain designs on the Boulevard. The unit at Confederation Square is more contemporary in style and should be used more extensively.

There are currently only 3 drinking fountains on the Boulevard. These are important amenities and should be increased in number.



*Drinking fountain: contemporary design at Confederation Square*

### 4.2.8 Poster Collars

Metal poster collars have been installed on selected traffic lights. These are provided for public use in a successful attempt to reduce the use of the lamp posts themselves for postering.

Poster collars are fabricated aluminum cylinders affixed to traffic lights. There are currently 42 collars, which are concentrated along Wellington, Elgin, Mackenzie and Sussex.



*Poster collar: Sussex Drive*

### 4.2.9 Snow Removal Markers

Removable poles with reflective tips are installed adjacent to street trees, lights, poles and other features to indicate the limits of snow-clearing. These are inserted into permanent holes in the concrete unit paving and successfully protect physical assets against damage during snow removal operations.

Snow removal markers are fabricated from green fibreglass. Over 2,000 are installed on Confederation Boulevard each year.



*Snow removal markers: Wellington Street*



### 4.3 A Family of Signage and Graphics

Various types of signage are used on the Boulevard top provide roadway and visitor information, orientation and wayfinding, and traffic regulations. These are carefully designed to ensure a coordinated presentation of information in a form that is unique to Confederation Boulevard. Colours are carefully controlled and all elements feature the maple leaf often in red.

#### 4.3.1 Wayfinding

Key wayfinding elements on the Boulevard include map models, which are made of precast concrete with inlaid sculptured bronze, and information kiosks with wayfinding maps. The bronze relief maps and information kiosks provide visitors with an overview of the Boulevard and its key destinations.

There are 7 map models and 14 information kiosks on the Boulevard.

#### 4.3.2 Attraction Signage

The Visitor Access Network (VAN) is a coordinated signage system, which is designed to direct visitors from the city’s outskirts to the national attractions in the capital core.

Signs are custom designed and mounted on 100 mm diameter poles that are painted CB grey. They feature pictogram-based imagery and employ, for enhanced visitor recognition, the same colours as the provincial tourism signage programs.

There are approximately 150 attraction signs on the Boulevard.

#### 4.3.3 Street Identity Signs

Custom designed street signs identify the municipal streets that make up Confederation Boulevard. The blade features a distinctive shape and the fonts and colours reflect CB standards.

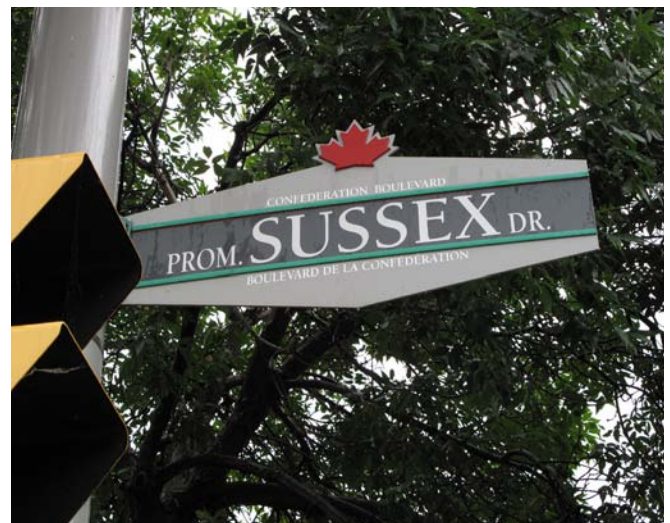
There are over 80 street identity signs on the Boulevard.



Map model: accessible wayfinding and orientation



Attraction signage



Street identity sign

**4.3.4 Interpretive Panels**

Interpretive panels identify important sites, describe the history of the Capital and explain the Boulevard itself. The panels often feature bronze reliefs further illustrating the panel content.

The panels consist of different components and materials: painted steel post and support, aluminum shrouds, bronze frame and sculptural elements and either high-pressure laminate or enamelled graphic panel.

The panels on the Boulevard reflect two interpretive programs:

**Phase 1 - Views and Streetviews (2002)**

The first phase consists of 10 interpretation panels that provide interpretation of some of the spectacular views found along Confederation Boulevard. All of the panels feature a horizontal bias. Each panel includes a custom sculpted bronze relief that further illustrates the panel content.

**Phase 2 - Sites and Institutions (2006)**

A second phase of panels consists of 15 panels that interpret key sites and national institutions located along the Boulevard. These panels feature two standard bronze header designs, one horizontal and one vertical. Unique bronze embellishments are added in the form of artifacts.

**4.3.5 CB Identity Standard Graphics**

The NCC has developed a standard signature and graphic presentation that is unique to the Boulevard. The palette includes colours, graphic elements, fonts and text treatment, and special graphic elements.

These are illustrated on the opposite page.



Interpretive panel describing the Confederation Boulevard banners



Interpretive panel with bronze relief



**Colours**

PMS 3298 PMS 340 White PMS 419 C PMS 417 C PMS 415 C PMS 413 C PMS 187 C PMS 340 C

NCC Banner Backgrounds (select most appropriate) Accents

**NCC band**

**CB Signature**

**Graphic elements**

Maple Leaf Accents  
Green Band Accent

Overview of graphic elements (NCC Graphic Guidelines, 2006)

## 4.4 Trees on the Boulevard

Deciduous street trees are an important feature of Confederation Boulevard. They not only provide shade and seasonal variation along the Boulevard in general, but they also reinforce the pastoral character of the Crown side particularly along the Grand Esplanade.

### 4.4.1 Tree-planting Details

Trees are planted both behind the sidewalk and within the paved sidewalks adjacent to the curb. Elm trees have been the preferred species for the Boulevard, drawing upon the historic images of Wellington Street lined on both sides by mature elms. While most of the original elms were lost, they are still the preferred species for new plantings with preference towards the Liberty Elm in particular along Wellington Street. Other species used on the Boulevard include maples, hackberry and ash.

Trees planted behind the sidewalks in open lawns tend to thrive while those in pavement have proven more difficult to maintain in good condition. Challenges include soil volume, compaction, drainage, irrigation and de-icing salt.

A number of solutions have been tested to improve conditions for trees in pavements. In the early phases of the Boulevard, trees were planted in contained pits with the tree pit paving supported on heavy steel grates to reduce soil compaction. In some cases, continuous tree pits were developed to increase the amount of topsoil available to trees.

The NCC continues to explore new planting techniques. CU Structural Soil is organically enriched gravel that supports plant growth, but can support pavements and other structures as well. The City of Ottawa is also using this technique with some success.

The use of soil cells is also being considered. Soil cells are like plastic egg crates that can be stacked and filled with topsoil. The cells are designed to support pavements and other structures while maintaining the friability of the topsoil. This relatively new method is currently being tested in other jurisdictions.

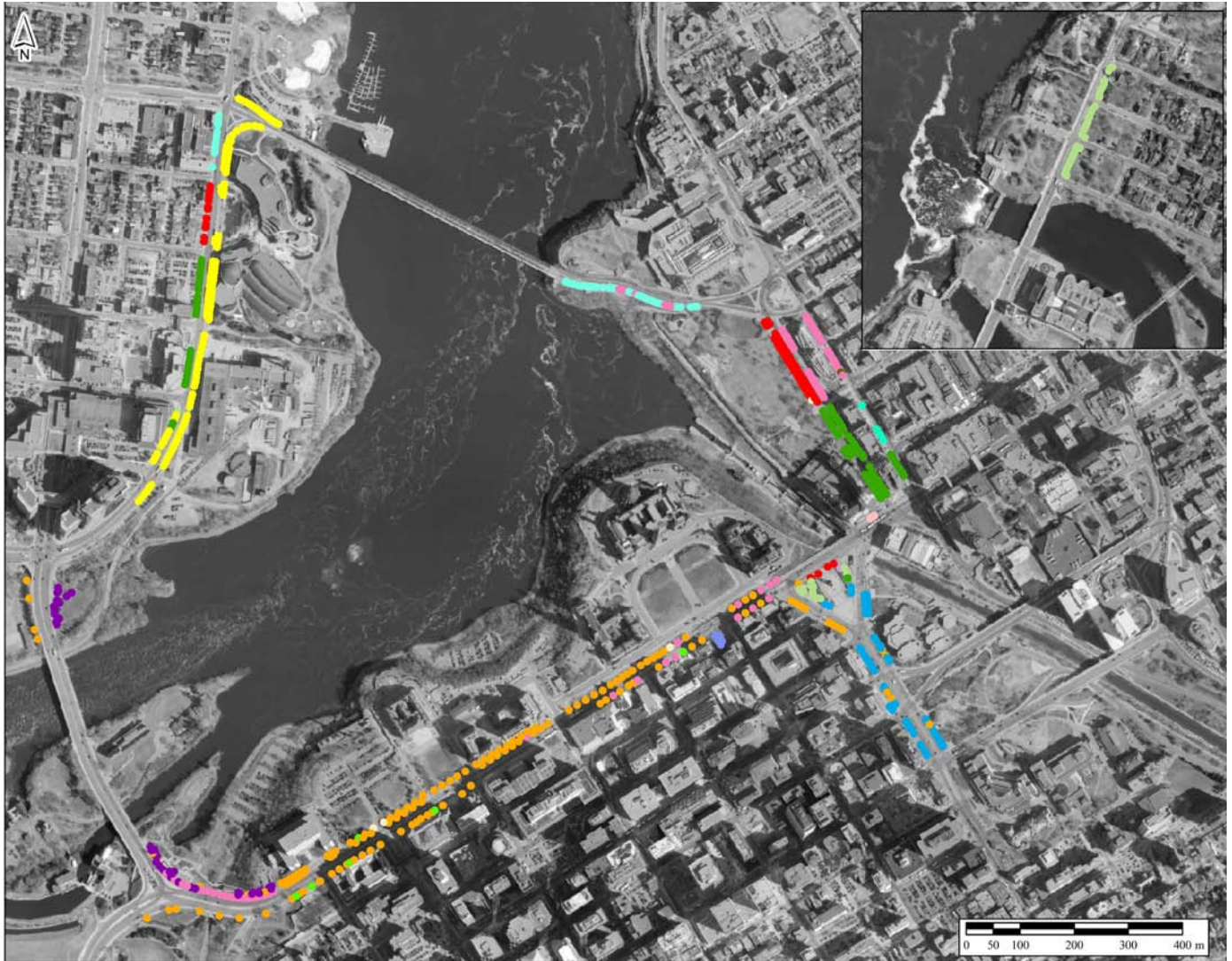


*Trees in paving and behind the sidewalk: Wellington Street*



*Soil cells being installed on the waterfront in Toronto*

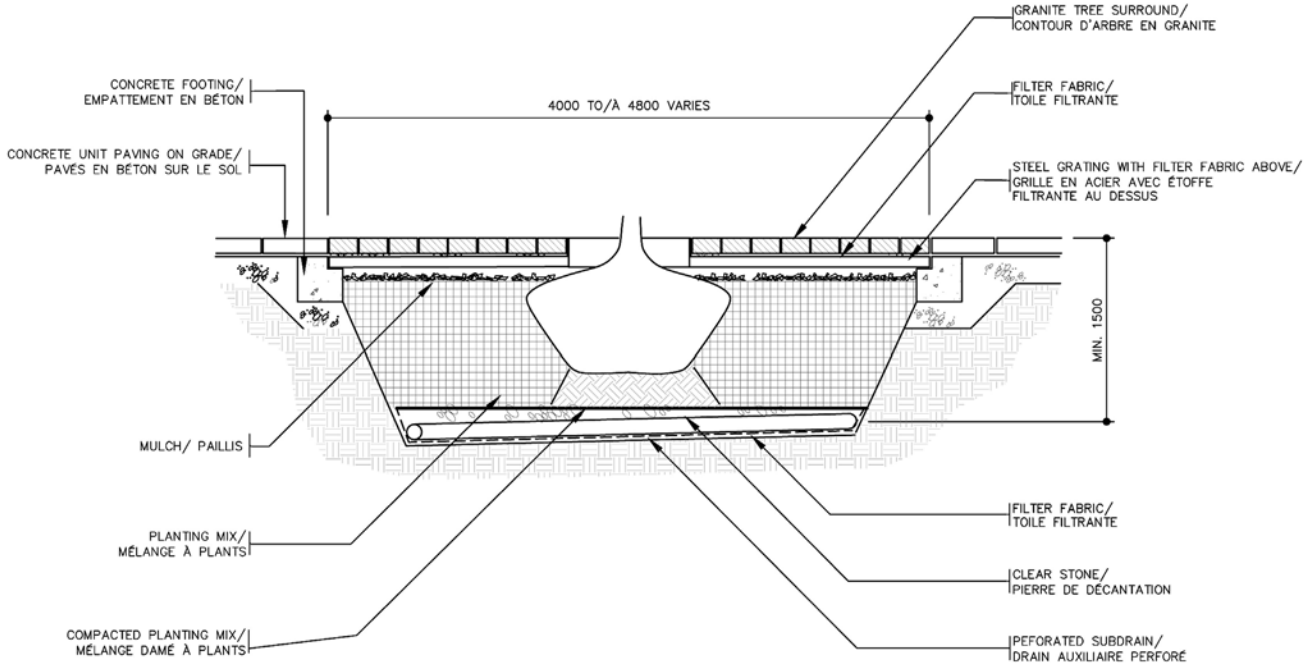




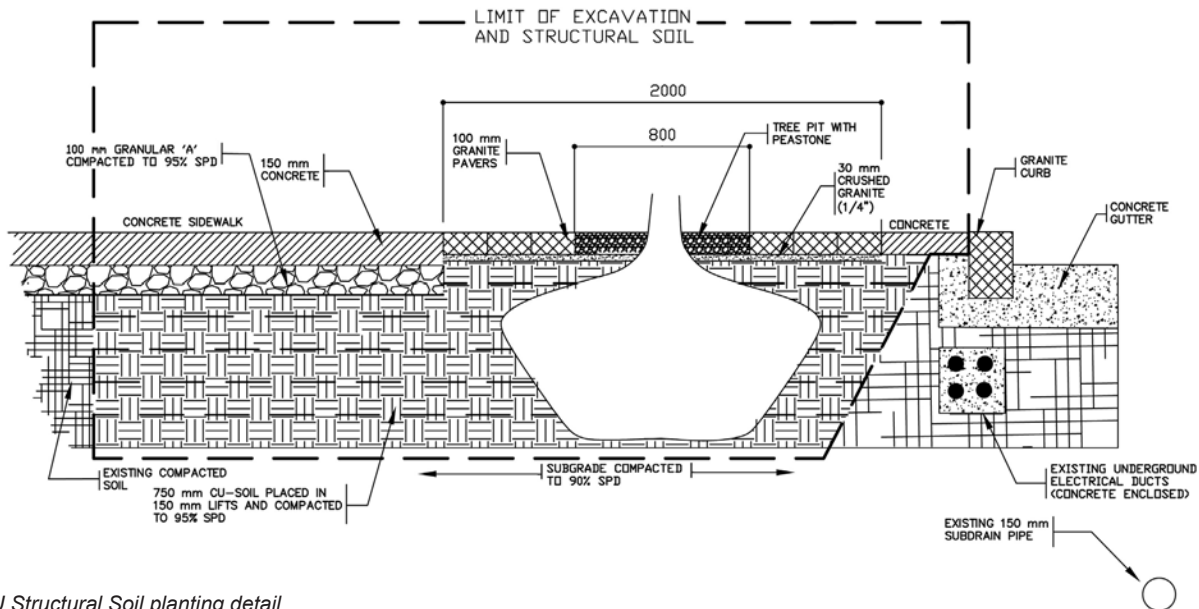
- |                                     |                                     |                                       |
|-------------------------------------|-------------------------------------|---------------------------------------|
| ● Ash - Red (Green) (41)            | ● Elm - Liberty (76)                | ● Maple - Freeman - Autumn Blaze (81) |
| ● Ash - White (3)                   | ● Elm - Siberian (4)                | ● Maple - Norway (129)                |
| ● Basswood - Littleleaf Linden (60) | ● Elm - White (137)                 | ● Maple - Red (1)                     |
| ● Beech - American (1)              | ● Hackberry (40)                    | ● Maple - Sugar (46)                  |
| ● Elm (3)                           | ● Lilac - Japanese - Ivory Silk (3) | ● Oak - Red (61)                      |
| ● Elm - Brandon (6)                 | ● Locust - Honey (8)                | ● Unknown (1)                         |

**Confederation  
Boulevard  
Boulevard de la  
Confédération**

*The distribution of trees by species*



Typical natural topsoil/metal grate planting detail



Typical CU Structural Soil planting detail



### 4.4.2 Granite Tree Pit Paving

Granite setts are used over the root zones of trees growing in paved areas. These are intended to provide a reasonably smooth walking surface while permitting some degree of water penetration.

The granite setts represent a shift in thinking from the first phases of Confederation Boulevard where the tree surround was indistinguishable from the surrounding granite paving.

Granite setts are 200 mm x 200 mm x 80 mm with colour to match granite curbs and borders.

### 4.4.3 Tree Base Treatment

Treatment at the base of trees in pavement has evolved over the years. Cast metal collars were used in the first phases. Later phases employed precast concrete collars. These were quite attractive, but were subject to spalling and snowplow damage.

The current practice is to fill the void around the tree trunk with coarse gravel. In addition to being clean and simple, this method permits some water infiltration and eliminates the collection of debris.



*Tree pit paving and metal collar: the first phases of the Boulevard*



*Granite paving and precast concrete collar on Wellington Street*



*Granite paving and gravel tree surround on Mackenzie Avenue*

## 4.5 Temporary Installations

Temporary installations provide a dynamic quality to Confederation Boulevard that animates and offers a continually fresh experience. Flags, banners, decorative lighting and floral displays reflect seasonal variation, topical themes and current events.

### 4.5.1 Flags

Flags are displayed along Confederation Boulevard to celebrate the arrival of important heads of state and other dignitaries. Canadian flags alternate with the flags of the country being honoured. They are mounted on the long arms of street lights lining the ceremonial routes. Flags on poles are the responsibility of PWGSC.

In addition, the NCC installs special mast-mounted flag groupings at selected locations along the Boulevard.

### 4.5.2 Banners

Rotating displays of almost 900 colourful banners enliven Confederation Boulevard during the summer months. Banners are designed according to a number of different programs. Some displays are themed to celebrate persons, groups, events or ideas. Other displays promote outreach and include banners designed by groups such as school children across the country.

### 4.5.3 Photo Exhibits

Photo exhibits are mounted during the summer in key locations along the Boulevard. These exhibits feature topical current or historical themes.

### 4.5.4 Christmas Lights

The Christmas Lights display (now using LED technology) is celebrated with an opening ceremony on Parliament Hill near the beginning of December. Originally limited to Parliament Hill, the display now includes several sites along Confederation Boulevard.

Installed on trees and shrubs on the Crown Side, lights are concentrated at the nodes and other strategic locations in order to focus the visual impact and facilitate economical installation.

### 4.5.5 Floral Displays

Floral displays play a prominent role along Confederation Boulevard and throughout the Capital. The Floral Program includes tulips and annual flowers displays in planting beds, and particularly in planters located on Crown Side along Confederation Boulevard. All displays are designed by the NCC landscape architects to be enjoyed by pedestrians as well as those in vehicles.

New approaches to the floral displays are being developed to refocus resources and reflect the structure of the Capital. The Capital Core including Confederation Boulevard will be the setting of the most colourful and intensive displays, using high quality plants set out in consciously designed arrangements. The colour red will be prominent in displays along Confederation Boulevard. These displays will receive higher levels of maintenance compared with other NCC lands.

The floral displays are managed by NCC staff, but installed and maintained by private contractors.



Flags and banners along Sussex Drive





Christmas lights display: Elgin Street



Floral displays in planters: Mackenzie Avenue



Banners on Sussex Drive



Photo exhibit: Sappers Bridge



## 4.6 Municipal and Other Features

Confederation Boulevard is made up of City-owned streets with heavy pedestrian traffic and high volumes of both private vehicles and public transit. For this reason, the Boulevard includes all of the normal furnishings, signage and other equipment typically found on municipal streets.

The physical reality of these features can compromise the distinct and dignified character that the NCC has striven to create for Confederation Boulevard. The unique image of the Boulevard is eroded by those features that are common to all other streets, and by those that are simply unattractive. Efforts are, however, being made to bring features into the Boulevard design family, and to reduce the impact of those that cannot.

**Traffic lights** - Fixtures are designed to municipal standards, but the poles and related control boxes are painted CB grey.

**Regulatory signage** - Signs are governed by the Ontario and Quebec Ministries of Transportation; the poles are 5.7 mm dia. and painted CB grey.

**Bus shelters** - Municipally owned and supplied, and fabricated to an NCC design. There is some concern that new digital advertisements may become standard and may impact the character of the Boulevard if not controlled.

**Fire hydrants** - Design, colour and location is mandated by municipal legislation.

**Telephones** - These are supplied and installed by Bell Canada, painted CB grey. Telephones on the Boulevard are gradually being phased out as mobile communication technology becomes more prevalent.

**Newspaper boxes** - These are supplied and installed by independent media, but not coordinated with CB themes. Locations are controlled by municipal license.

**Tour bus ticket kiosks** - Supplied and installed by independent operators, these kiosks are not coordinated with CB themes. Locations are controlled by municipal license.

**Information kiosks** - Formally referred to as CB Information Map Columns (or “Colonne Morris”), these features are permanent and weather-protected installations provided at strategic locations along the Boulevard. Privately owned and contracted to the NCC, the kiosks are constructed of steel and plexiglass with CB colours and maple leaves.

The information kiosks provide wayfinding maps and information about events and destinations in the National Capital.

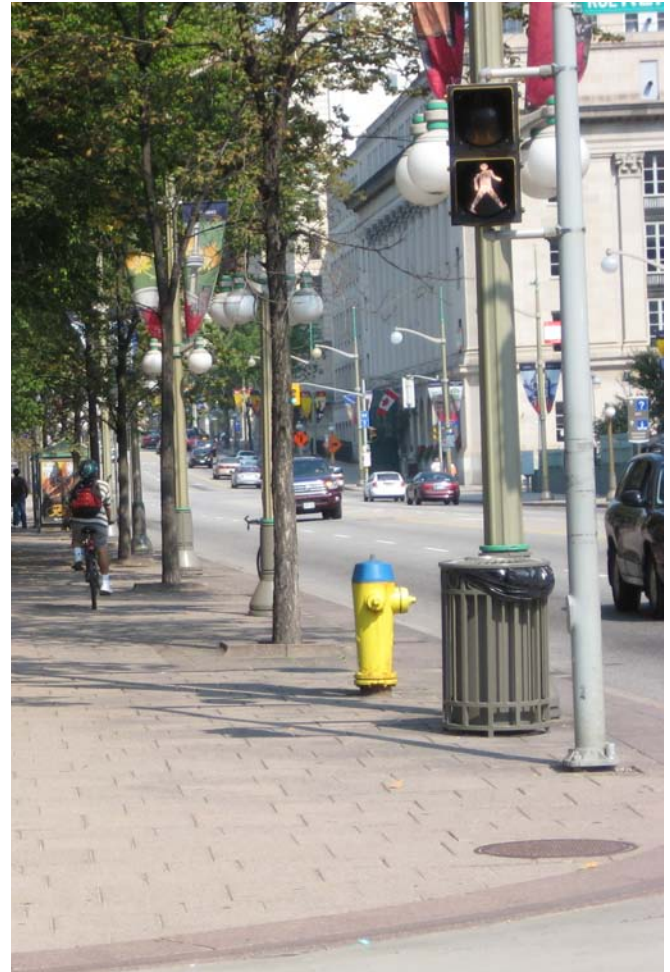


Municipal traffic signals and signage: Wellington Street





Bus shelter: Elgin Street



Fire hydrant: Wellington Street



Tour buses and ticket kiosks: Elgin Street



Newspaper boxes: Elgin Street



Information kiosk: Mackenzie Avenue





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## PART 5 - STEWARDSHIP OF CONFEDERATION BOULEVARD

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***Coordinated Management  
Design and Development  
Programming the Capital Experience  
Maintenance and Operations  
Activity on the Boulevard***

While the construction of Confederation Boulevard has been quite enough of a challenge, its ongoing maintenance and management will demand continuing diligence and discipline. This section sets out guidelines for design development, programming and communication, as well as maintenance and operations.

An essential component of this will be the ongoing challenge of coordinating the efforts of the various agencies (both within the NCC and without) that have an interest in Confederation Boulevard.

## Introduction

*“Give the completed construction of Confederation Boulevard and its maintenance a high priority.”*

### **Canada’s Capital Core Area Plan, 2005**

As the construction of Confederation Boulevard draws to a close, attention will necessarily shift to ongoing maintenance and operation. Tremendous energy and resources went into realizing the Confederation Boulevard ambition, and a similar level of effort will be required if that ambition is to remain alive and vital in the years to come.

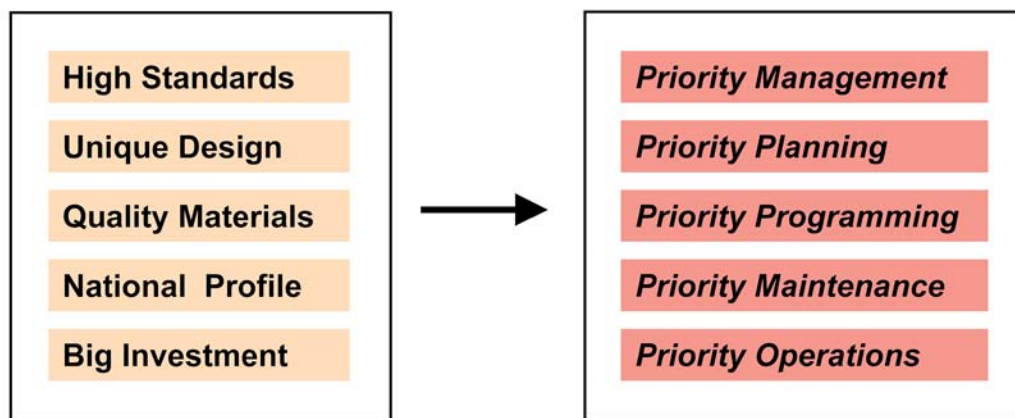
Much of the effort will be required to maintain and operate the Boulevard on a day to day basis. Cleaning, repairing, watering, mowing, painting, fixing, planting, changing light bulbs, setting up for events - these regular activities are essential and will become increasingly important as materials and components age over time.

Effort will also be required to plan for the inevitable replacement and rehabilitation of components as they eventually wear out. Life cycle planning will be a comprehensive, long-term program that needs to be supported by adequate and predictable funding.

Although nearly finished in one sense, Confederation Boulevard will never be a static artifact that never changes. Rather, it will remain a dynamic organism that will evolve in response to new circumstances, priorities and pressures. Considerable effort will be necessary to ensure that the essence of the Boulevard is not overwhelmed by these pressures.

Pressures for change are visible even today. Reflecting the natural evolution of the city itself, these pressures stem from the need to improve public transit and accommodate cyclists, from changing security priorities, from new technologies such as digital advertising, and from future private and institutional developments on both sides of the river.

There will also be budgetary pressures, and these pressure will only increase as the focus shifts from development to maintenance. While it is hard enough to secure funds to build something new, it is twice the challenge to fund its ongoing maintenance. The temptation to save money by reducing maintenance and rehabilitation budgets will only compromise a great asset, and negate both the promise and investment inherent in Confederation Boulevard.



*Confederation Boulevard warrants intensive stewardship*



**National Capital Commission:**

- Owns Portage Bridge and approach roads;
- Owns the surface finishes of the Boulevard;
- Sponsors and finances Boulevard streetscape development;
- Sponsors and finances temporary floral, lighting and banner displays;
- Maintains Boulevard trees, lighting, pavements and other landscape structures;
- Sponsors and manages special events.

**Public Works and Government Services Canada:**

- Owns and maintains federal buildings, parking lots and roads within the Crown Territory and along the Boulevard, including Alexandra Bridge.

**City of Ottawa:**

- Owns the Boulevard streets on the Ottawa side (except as noted above);
- Responsible for traffic signals and signage;
- Responsible for policing and security in Ottawa;
- Responsible for snow and ice removal;
- Manages utility installation and repair;
- Issues vendor and busker permits;
- Issues events permits.

**City of Gatineau:**

- Owns the Boulevard streets on the Gatineau side (except as noted above);
- Responsible for traffic signals and signage;
- Responsible for policing and security in Gatineau;
- Responsible for snow and ice removal on the Town Side;
- Manages utility installation and repair;
- Issues vendor and busker permits
- Issues events permits.

## 5.1 Coordinated Management

An overseeing planning and administrative body should be established to coordinate the agencies that plan, maintain and operate Confederation Boulevard.

The ongoing development, maintenance and management of Confederation Boulevard will be challenged not only by the magnitude of the undertaking, but also by the complex proprietorship that governs decision-making. The adjacent sidebar identifies the key stakeholders and their respective responsibilities.

Despite the complexities of the Boulevard's ownership and management structure, things do get done. By virtue of a variety of both formal and informal agreements and understandings among the stakeholders, not to mention considerable good will, the Boulevard was constructed and is being maintained and operated in a reasonably effective manner.

However, the activities and initiatives undertaken by these various agencies are not always fully coordinated. Conflicting objectives and priorities can lead to duplication, inefficiencies and incompatibilities. There is also the potential for future problems as individuals retire or otherwise move on, and as new people who lack a historical perspective and bring different priorities take over.

A part of the solution is to adopt a consolidated vision for Confederation Boulevard that clearly confirms what it will look like and how it will be used. Such a vision is the intent behind the first four parts of this report.

A second part of the solution is to establish a formal NCC management group to oversee, direct and coordinate the activities of the agencies responsible for the Boulevard. This group would consist of senior staff representing all relevant branches within the NCC.

Coupled with the establishment of a senior management group within the NCC would be the development of more formal links with the Cities of Ottawa and Gatineau. The present liaisons that are largely based on personal history and friendly contacts will begin to fray as people retire or otherwise move on. Formal links will ensure more effective communication in the short term and greater continuity over time.

**Capital Planning Branch**

- Planning and Transportation
- Federal Land Use Approvals & Municipal Coordination
- Federal Design Approvals, Heritage and Archaeology

**Real Estate Management, Design and Construction Branch**

- Design and Construction Division
- Architecture Section
- Landscape Architecture & Urban Design Section
- Engineering Section

## 5.2 Design and Development

### 5.2.1 Project Planning and Design

The planning and construction of Confederation Boulevard was accomplished through the concerted efforts of the Capital Planning (CP) Branch and the Real Estate Management, Design and Construction (REMDC) Branch. These groups undertook major planning initiatives, coordinated consultant input and design development, and managed construction contracts.

Now that Confederation Boulevard is nearing completion, the roles of CP and REMDC will shift from implementation to the monitoring and evaluation of proposals put forward by others. It is anticipated that many of these proposals for change will come from the Cities of Ottawa and Gatineau as responses to new initiatives for accommodating public transit and cycling infrastructure. While important objectives, these cannot be realized at the expense of the quality and character of Confederation Boulevard, and of the Grand Esplanade in particular.

It will be the continuing role of CP and REMDC to work with stakeholders to find solutions to new problems that support rather than erode the basic character of the Boulevard.

### 5.2.2 Federal Approvals

The National Capital Act mandates the NCC to review and approve changes on NCC or on any federal lands in the Capital region including Confederation Boulevard. To facilitate this responsibility, the NCC has developed rigorous review and approval procedures to ensure both high quality planning and design, and the protection of the natural environment and built heritage.

Two forms of approval have been developed. The Federal Land Use and Design Approval (FLUDA) deals with land use, master planning and project design for construction. The Federal Land Transaction Approval (FLTA) deals with real estate and land transactions of all sorts. Whether a proposed project requires the full review and approval process is at the discretion of the Vice President of the Capital Planning Branch.



The Board of the National Capital Commission is the approval authority for significant projects. When reviewing plans and projects of major significance, the NCC will seek advice from its Advisory Committee on Planning, Design and Realty (ACPDR). This body, which meets every two months or so, is made up of planning and design professionals from across the country.

The approval process is not complete until a Federal Approval document for the project has been received by the proponents. Any subsequent modifications to the approved design(s) must be submitted to the Vice President of the Capital Planning Branch for review and approval prior to implementation.

Proponents of works on or adjacent to Confederation Boulevard are advised to contact the CP Branch early in the project to ensure that the review and approval process is coordinated with the project schedule.



*Rue Laurier under construction, 2000*

### **5.2.3 Environmental Assessment**

Environmental Assessments are undertaken to ensure that new projects comply with the Canadian Environmental Assessment Act (CEAA) and the NCC's Environmental Strategy. The NCC's Environmental Strategy promotes a greener Capital, compliance with environmental legislation, the provision of environmental benefits, and the protection of all important natural and built assets.

Whether an Environmental Assessment is required is determined first through an Environmental Review, which is required for all development and land use proposals that require Federal Land Use or Land Transaction approvals.

### **5.2.4 How to Apply for Approval**

All proposed projects or land transactions should be discussed with staff at the CP Branch who will advise on the approval process. Staff will indicate whether a proposal requires a formal approval by the NCC Board. Further guidance is also provided on the NCC website at [www.canadascapital.gc.ca](http://www.canadascapital.gc.ca).

Completed form and attachments are sent to:

Vice President, Capital Planning Branch  
National Capital Commission  
202-40 Elgin Street  
Ottawa, Canada K1P 1C7  
Fax: 613-239-5302

There is a fee of \$2,000 (plus HST) for reviewing applications for federal land use and land transactions. There is no application fee for the review of design projects.

***Capital Experience, Communications and Marketing Branch***

- Public Programming Division
- Programming and Celebrations
- Capital Interpretation
- Commemorations and Representation
- Industrial Design and Environmental Graphics
- Production Services
- Capital Marketing Division
- Strategic Communications
- Digital Communications, Outreach and Youth Programs

## 5.3 Programming the Capital Experience

The physical reality of Confederation Boulevard is made meaningful to people by an interpretive overlay that promotes the functions, symbols and meanings of the National Capital. It is a key aim of the NCC to not only make the Capital a beautiful place, but to also communicate and reveal why it is important and how it represents the values of all Canadians. This is accomplished through a variety of programs that focus on both events and physical installations.

### ***5.3.1 Programming and Events***

The NCC promotes and manages a series of significant public events on the Boulevard that celebrate landmark dates, reflect the change of seasons and animate the Capital Core. These include Canada Day, Winterlude, Sound and Light Show and the Christmas Lights Opening Ceremony.

These events complement the serious business of running a nation, which is the core purpose of the National Capital, and introduce a sense of festivity that reflects another dimension of the Canadian reality.

### ***5.3.2 Wayfinding Signage***

Because the core area of the Capital Region is a prime destination for visitors, there is a great need for “wayfinding” signage to direct and inform. As part of its mission to help people discover the institutions, landmarks and symbols in the core area, the NCC has instituted a system of signs and maps to help visitors find their way to the core area and along Confederation Boulevard.

The design, fabrication and installation of signage systems in the core area is the responsibility of Industrial Design and Environmental Graphics (IDEG).

### ***5.3.3 Interpretation***

The direct presentation of information about the function, character and history of the National Capital enriches the visitor experience and increases general public awareness.





*National War Memorial*

The installation of plaques and interpretive banners along Confederation Boulevard has been to date quite successful. The plaques are enduring sources of information while the banner programs provide dynamic presentations of topical and current celebrations.

Rather than relying on more plaques, future interpretation programs could utilize digital/wireless technologies that would convey information with minimal “footprint”.

### **5.3.4 Commemorations and Public Art**

Commemorations in the National Capital give physical meaning and presence to Canadian values of national importance. The NCC is charged with implementing proposed commemorations once they have been thoroughly vetted, approved and funded. Commemorations are usually designed through a public competition process administered by the NCC in collaboration with the promoting agency.

Confederation Boulevard is a favoured venue for commemorations because of its visual prominence and proximity to the National Symbols. The NCC has identified potential sites for commemorations on the Boulevard, which are reserved for installations that reflect only subjects, events and values of the highest national importance. These sites have been carefully identified to reinforce the basic structure and character of the Boulevard.

The NCC is currently developing policies to encourage the installation of public art pieces as integral components of the public realm. An important complement to commemorations, public art can add distinction to the National Capital and Confederation Boulevard in particular, celebrate artistic talent and creativity, and stimulate cultural tourism.

### **5.3.5 Communication Media**

In order to promote the National Capital and Confederation Boulevard, the NCC makes full use of many means of communication and interactive media. The purpose is to enrich the visitor experience and to extend the Capital Experience beyond the Capital.

Even in the face of new technological advances, conventional print and media will remain an important component of the program. Pamphlets, brochures, posters and maps have immediate appeal to all people, and have the added advantage of being easily stowed in the back pocket. A relevant example is the ***Discover the Heart of the Capital*** brochure that is dedicated to Confederation Boulevard.

Digital media of all kinds are becoming increasingly important to the NCC’s communication strategy. The primary reason for this is the ability to reach a much larger audience including those in distant parts of the country (or the world) who might not have thought of visiting the Capital or who might not ever be able to do so. The NCC website ([canadascapital.gc.ca](http://canadascapital.gc.ca)) is an important component of this effort, as are webcams which can provide a vivid and current experience of the Boulevard. A more recent technology is Quick Response (QR), which uses bar codes to stimulate cell phone-based narratives.

The Capital Information Centre (CIC) offers information on destinations and landmarks throughout the National Capital. For a number of years, it has occupied a highly visible location on the Boulevard across from Parliament Hill where has served many thousands of visitors. The NCC is currently planning to relocate the CIC to an off-site location and rely more heavily on digital media.

In the new media, keeping current is everything. All information sources should be kept up-to-date and should be simple, complete and accessible.

***Environment Capital Lands and Parks Branch***

- Environmental Management and Protection Division
- Environmental Services
- Official Residences Division
- Urban Lands and Transportation Division
- Land Management
- Maintenance Operations Management
- Asset Rehabilitation Management

## 5.4 Maintenance and Operations

### ***5.4.1 High Standards/High Expectations***

The very role and character as a central showpiece of the National Capital demands that Confederation Boulevard be maintained in top physical condition. It should look good at all times, and it should also efficiently accommodate the various events and activities that occur on and around it. Going forward, Confederation Boulevard is too important and represents too great an investment to compromise on maintenance and Life Cycle Management.

The shift that is now occurring from development to ongoing maintenance and operation means that standards commensurate with the status and importance of the Boulevard should be developed and applied. These standards will reflect the established vision of the Boulevard as a key component of the National Capital, and the expectations implicit in that vision.

The key ingredients of an effective maintenance and operations program will be collaboration among partners and stakeholders, effective contract management and adequate funding.

A maintenance/operations plan should be developed in which expectations are defined, a scope of work and budget established, and long term funding secured.

### ***5.4.2 Life Cycle Management***

An effective ongoing maintenance program is far more economical in the long run than the premature replacement or rehabilitation that can result from neglect.

Nevertheless, an effective maintenance strategy needs to be coupled with a long term strategy for the eventual replacement of furnishings and components. To achieve this, a comprehensive funding model should be developed that best balances ongoing maintenance costs against replacement costs.

### ***5.4.3 Asset Management***

Maintenance, operations and long term planning all depend upon ready access to a complete and current



inventory of all of the Boulevard's equipment, furnishings and components. Lacking such an inventory can only lead to inefficiency, duplication and wasted effort. The NCC maintains two types of inventory:

- **Asset Information Management Tool (AIMT)** - All of the physical components on the Boulevard are mapped using GIS technology. These maps are invaluable for developing maintenance programs, managing maintenance contracts, and for Life Cycle Management planning.
- **Integrated Asset Management Information System (IAMIS)** - This tool includes a complete inventory of mobile assets including everything from vehicles and heavy equipment to furniture, tools and audio/visual equipment. The inventory maintains records of assets in storage and in use.

These inventories will only be useful if they are comprehensive, up-to-date and accurate. This requires a commitment that is reflected in adequate planning, funding and staffing.

Effective asset management also depends upon timely commissioning of new projects and major repairs. All drawings, specifications and operating manuals should be handed over by proponents upon completion of work, and catalogued and made available to those who will need them in the future.

#### 5.4.4 Maintenance Contracts

The maintenance of Confederation Boulevard is undertaken by a combination of municipal crews and private contractors.

All regular maintenance on Confederation Boulevard (and other NCC properties) is undertaken by private contractors. This includes general cleaning, waste management, looking after floral displays, grass mowing, tree maintenance, repairs to lights and furnishings and quick clean-up after events.

In addition to municipal efforts, Confederation Boulevard is now maintained through at least four different contracts issued and administered by the NCC.

Three separate maintenance contracts cover trees and floral displays, street lights and other fixtures, and flags and banners.

The largest contract covers pavement and furnishings maintenance, general clean-up, waste management, grass-cutting, and some snow and removal. This single contract covers all of the Capital Core landscapes including the Boulevard.

A concern has been raised that since the Boulevard is one of many components of this larger contract, main-



*Floral displays adjacent to Confederation Boulevard*

## 5. Stewardship of Confederation Boulevard

tenance standards are not sufficiently high. In fact, existing contracts set out performance standards that specify a higher level of maintenance for the Boulevard than for other landscapes (except for Parliament Hill itself). The alternative, a stand alone contract for Confederation Boulevard, has been considered and rejected because of efficiencies inherent in managing multiple contracts.

The best maintenance is provided by people who care. Good contract management will not only look after all the contract legalities, but also inspire a strong sense of pride and ownership. Effective maintenance staff members understand the significance of the undertaking and feel that their contribution is important.

### 5.4.5 Snow Removal

Snow and ice control is one of the most important maintenance activities on the Boulevard, and the one that exacts the most impact. Essential for maintaining personal safety and for keeping the city on the move, snow and ice control results in considerable physical damage to trees, pavements, lights, poles and furnishings. This damage is often unsightly and expensive to repair.

Snow and ice control is managed primarily by the Cities of Ottawa and Gatineau. Municipal staff undertakes most of the snow clearing and salting on city streets including Confederation Boulevard. The Cities contract out some removal operations to private contractors.

Only the Crown side of Confederation Boulevard on the Quebec side is cleared through private contract.

Although various measures have been considered over the years to protect the Boulevard, a certain amount of damage from snow removal equipment is unavoidable. At issue is undertaking timely repairs and replacements, and reducing damage.

Arrangements are now in place whereby the Cities compensate the NCC for a certain portion of the cost of repairs. Although negotiations recur every year, the principle is that the Cities compensate the NCC for the value of the same damage to standard municipal furnishings and equipment; the NCC accepts responsibility for costs above that base amount, recognizing the higher value of Boulevard components.

A number of measures can be undertaken to reduce the amount of damage:

- Readily movable elements including planters, benches, waste receptacles and bike racks should be removed from areas regularly plowed, especially where space is limited. The green snow removal markers are quite effective and should continue to be used.
- Equipment should be employed that is appropriate to the task. While lightweight machines and rubber blades have been proposed for many years, the fact is that the Cities have standard equipment that is used on all city streets including the Boulevard.
- Machine operators should also be well-trained and familiar with the Boulevard's particular challenges. While it would be preferable to have the same individual looking after the same portion of the Boulevard, staffing and work schedules (not to mention storm schedules) seldom facilitate this on a regular basis.

From time to time, consideration has been given by the NCC to managing specific snow removal contracts for the Boulevard in order to realize a higher level of service. While attractive, this proposal has been rejected because of the difficulties involved in coordinating private contractors and municipal staff working in close proximity. The current system also allows for easier cost recovery and requires fewer management resources.



Winter maintenance



### 5.4.6 De-icing Agents

De-icing is a constant issue. Some de-icing, at least, is necessary and effective in our climate. However, salt, the most commonly used de-icer, damages trees, lawns and pavements, and contaminates storm water runoff. While salt is more economical than other products, its total cost should include the environmental impacts brought on by its use.

Concerted efforts should be made to find safer alternatives to salt. Consideration should be made to implementing pilot projects on Confederation Boulevard.

### 5.4.7 Street Trees

Maintaining the curbside street trees on the Boulevard has been an ongoing challenge for the NCC. These trees have tended to suffer from inadequate soil volume, soil compaction, irrigation problems (too much or too little), lack of oxygen, salt contamination and snowplough damage. Replacement is consequently frequent.

It is, however, better to maintain trees than replace them. Since they take such a long time to grow, actively maintaining a larger tree is much preferred over letting it go and replacing it with a small caliper tree.

Irrigation was tested in the early development phases but abandoned due to high capital and maintenance costs. Trees are now watered by truck.



Summer maintenance

### 5.4.8 Pavements

Boulevard pavements do require maintenance if they are to continue to look good and perform well.

Good materials properly installed last a long time. The concrete unit paving that has been used since the early 1990s (Megapaver by Permacon) has performed extremely well and has required little maintenance. This is not only due to the quality of the material itself, but also because the installation was carefully designed and managed. The granite paving used in the first construction phases did not perform well and is being replaced with concrete unit paving.

The poured-in-place concrete sidewalks that characterize most of the Town side of the Boulevard are more difficult to maintain to Boulevard standards. While economical and very practical, concrete does tend to stain and discolour as it ages. This becomes all too evident when sidewalk repairs introduce patches of fresh new concrete.

The Cities of Ottawa and Gatineau maintain the concrete paving on the Town side of the Boulevard.

### 5.4.9 Utility Repairs

Like other city streets, Confederation Boulevard streets are occasionally impacted by utility repairs. Normally undertaken by or on behalf of the Cities of Ottawa or



Tree pit repair



Floral displays



Street lights

Gatineau, the repairs (and associated reinstatements) typically comply with industry standards, which are not necessarily the standards of Confederation Boulevard.

To maintain the Boulevard in good condition, utility repair contracts should stipulate reinstatement standards that equal or exceed the existing conditions. These contracts should also include assertive contract management and diligent follow-up.

#### **5.4.10 Metalwork**

The decorative metalwork on Confederation Boulevard, which has generally performed well, is the responsibility of the NCC. Maintenance work includes refinishing and repairs as required and is handled through the primary maintenance contract.

#### **5.4.11 Floral Displays**

The floral displays along the Boulevard, including planters and hanging baskets, provide highlights of colour that many enjoy.

These displays do, however, require considerable maintenance including watering, weeding and replacement. This maintenance is labour-intensive and sensitive to changing weather patterns. Particular challenges surround the issue of vehicle access by contractors. Usually necessary to complete the work, vehicles are often required to block the curb lane or drive on the Esplanade itself.

New techniques such as the use of natural water sources and natural pest controls are consistent with the NCC's green policies.

#### **5.4.12 Street Lights**

On Confederation Boulevard, there are currently over 800 light poles and over 2800 light fixtures, all of which are maintained by the NCC. Simply replacing burnt out light bulbs is a major undertaking.

The NCC shares the cost of power with the Cities of Ottawa and Gatineau. The Cities reimburse the NCC for the estimated power that would have been used to illuminate the streets by conventional fixtures.



A major challenge in maintaining the fixtures is inherent in their unique design. There are well over 100 moulds necessary to produce all of the components. These have to be stored properly and catalogued for easy access when required.

#### 5.4.13 Furnishings

Benches, waste receptacles, bike racks, bus shelters and other furnishings require periodic maintenance.

Furnishings should be maintained to a very high level on the Boulevard for the very reason that people touch and come into close contact with these elements.

#### 5.4.14 Waste Management and Recycling

Frequent and thorough litter removal is a basic maintenance requirement on Confederation Boulevard and is looked after by various stakeholders. OC Transpo maintains the waste receptacles and cleans bus shelters and the area around the bus stops. Elsewhere on the Boulevard, waste management is carried out under contract managed by the NCC.

In line with NCC sustainability objectives, a curbside recycling program should be examined. This is a challenging undertaking attempted in many other jurisdictions. Issues include sorting and collection, and the design of the receptacles themselves, which are all too often large and unattractive.



Furnishings

#### 5.4.15 Signage

Signage is maintained by various stakeholders according to ownership. Coordination is required to ensure that right poles and fittings are used when replacements are required.

Where possible, signs should be in keeping with the character of the Boulevard. An issue has arisen regarding new municipal street signs at major intersections. These large blue street signs may improve wayfinding for motorists, especially those unfamiliar with the Capital, but they are visually dominant and out of character with Confederation Boulevard.

The design and implementation of new signage systems should be coordinated with all stakeholders and approved by the NCC.

#### 5.4.16 Vandalism and Graffiti

Vandalism and graffiti are serious problems in many jurisdictions, but has been manageable on Confederation Boulevard. One of the reasons may be the relatively high level of maintenance already in force on the Boulevard. Experience has shown that the best guard against vandalism and graffiti is, in fact, a high level of general site maintenance coupled with rapid response when there is a problem. Anti-graffiti coatings may be effective, but should be thoroughly tested to ensure that their visual character is satisfactory.



Municipal street signs

## 5.5 Activity on the Boulevard

Commercial and promotional activities can be accommodated on Confederation Boulevard without compromising the dignity of the National Symbols. To achieve this, they should be encouraged on the Town side of Confederation Boulevard, and discouraged on the Esplanade.

### 5.5.1 Event Management

Both large and small events are frequently held in the parks and open spaces associated with Confederation Boulevard. These often require considerable amounts of equipment including barricades, temporary seating, audio/visual equipment, power panels, sanitary facilities and other furnishings. In addition, planters, benches and other Boulevard furnishings sometimes need to be temporarily removed.

While these events bring life and vitality to the Boulevard, they can impact the quality of experience for those not involved. Efforts should be made to reduce the impact of large events by setting up efficiently and cleaning up quickly after the show.

### 5.5.2 Events Permits

Permits are issued by the Cities of Ottawa and Gatineau for events on the Boulevard.

The NCC is charged with issuing permits for concerts, festivals, performances and other events sponsored by external groups and held on NCC property.

On the Boulevard, however, events permits are issued by the Cities of Ottawa and Gatineau, who own the streets. The NCC carries little responsibility for managing events on the Boulevard simply because it owns very little of the land. To ensure that minimum Boulevard standards are upheld, the Cities should continue to collaborate with the NCC in the selection and timing of events.

### 5.5.3 Land Access Permits

Land access permits on the Boulevard are issued by the Cities of Ottawa and Gatineau. Land access permits



Strikes and protests

on the Boulevard relate primarily to sidewalk access for maintenance and construction vehicles.

The NCC is responsible for granting land access permits on all property it owns in the National Capital. As mentioned above, the NCC actually owns very little of Confederation Boulevard.

### 5.5.4 Strikes and Protests

Strikes, protests and other unofficial events regularly occur on Parliament Hill and at other key symbolic sites in the National Capital. Some of these events involve processions along Confederation Boulevard. While sometimes noisy and perhaps inconvenient at times, these activities are acceptable in our society as long as they pose no serious risk to personal safety. Moreover, they are only to be expected in the National Capital, which is the seat of the federal government.

However, due to the numbers of people involved and their sometimes adversarial nature, these events can result in negative impacts on the Boulevard. Postering on trees and street lights, littering, trodden lawns and plantings, and the informal use of NCC power outlets are common consequences.

Being largely unscheduled and sometimes unpredictable, these events have to be taken in stride. Rapid clean-up and repair are the only reasonable responses.





*Buskers and vendors*

### **5.5.5 Buskers and Vendors**

The Cities of Ottawa and Gatineau are responsible for issuing permits to vendors and performers. These activities should be supported as they help animate the Boulevard and provide basic amenities to passersby.

These activities can be accommodated on Confederation Boulevard without compromising the dignity of the National Symbols. To achieve this, they should be encouraged on the Town side of Confederation Boulevard, and discouraged on the Esplanade.

### **5.5.6 Advertising and Sponsorship**

Advertising in the public domain is very common due to the high visibility it offers and is frequently seen on bus shelters and information kiosks, and other furnishings. While it is a source of revenue for cities, on-street advertising and corporate sponsorship can compromise the visual quality of the urban environment if not properly controlled particularly within character landscapes and heritage districts. The NCC Sponsorship Policy limits corporate sponsorship within the National Capital to temporary events and programs. Quebec City has gone further, prohibiting signage and advertising in bus shelters and others permanent amenities throughout special districts such as “Vieux Québec”.

Confederation Boulevard is a symbolic, cultural landscape that protects our national symbols, attractions and sites as a legacy for future generations. The proper present and future branding of this dignified ceremonial

route, used by visiting dignitaries to our nation’s Capital, is therefore paramount.

As the agency responsible for the stewardship of Confederation Boulevard, the NCC must protect the dignity and symbolic importance of the National Symbols. Confederation Boulevard unites many of these key attractions through a unique and distinctive streetscape vocabulary in which third party advertising is limited to bus shelters and kiosks. Attempts have been made in the past to better control advertising in the City’s bus shelter program along the Boulevard with only limited success. Further discussion with municipalities should be pursued to establish a more appropriate advertising program for the bus shelters. Advertising of public programs, attractions, active mobility programs and services which help project the Capital image and branding would better serve NCC objectives.

Opportunities for integrating the new Capital Branding Program (2011) and other promotional initiatives should be encouraged as a means of raising the visibility and stature of the Capital image within the core area.

As the focal public realm of the National Capital, Confederation Boulevard must retain a character which is distinctive from the surrounding municipal fabric. As such, product branding and corporate identity has over the years, been restricted to prevent the proliferation of advertising in the core area. Corporate sponsorship programs, including property naming rights, should be discouraged along the Boulevard to prevent the dilution of it’s primary purpose and objectives. The NCC has adopted a Signage Policy and signage design guidelines (2011) as well as a Sponsorship Policy which should be employed to guide the decision making process.

With the advent of outdoor digital advertising and increased interest in revenue generating sponsorship programs, design review and controls will be paramount with any new street furniture programs, municipal or federal, where advertising or sponsorship initiatives impact the visual quality and character of the Boulevard.

Posting of temporary ads and posters on the Boulevard is accommodated by the pole collars provided at most intersections for that purpose. Posting elsewhere is prohibited under municipal by-law and should be discouraged through rapid removal and immediate clean-up.





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## CONCLUSION

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***“Landscape is like an open book from which one can read a story of those who live there.”***

Confederation Boulevard tells a story of the nation that built it. As a public place, this ceremonial route accommodates a full range of activities from pedestrian strolling and viewing to political protest and statement. As an urban design device, the Boulevard sets off and links legal and cultural institutions of the highest national importance. One can read of the emergence and development of a country that is relatively young, although one founded on principles and values that have evolved over many centuries. The Boulevard can be read as a story of a modest and proud nation that treasures democratic principles, social justice and political freedom.

The Boulevard honours the past and speaks of it to those who read it through the contemplation of commemorations and public art, who breathe the Canadian

experience by participating in programs and events, or who visit the federal institutions located on the Crown Side of the Boulevard. Confederation Boulevard also embraces the future; the book is never finished and next chapters will be written. The landscape will evolve over time in response to new priorities and pressures, many of which cannot even be imagined at this time.

Confederation Boulevard speaks of our commitment to environmental responsibility, the accommodation of all abilities, the promotion of Canadian culture and the creation of vibrant public spaces. Through its upkeep, we will tell the story of our commitment to what is Canada’s Main Street, the setting of our National Symbols. Adequate resources directed in a focused and disciplined fashion will ensure that this urban landscape is a source of pride, meaning and value to the Nation’s Capital and is showcased to the rest of the country and to the world for many generations to come.



